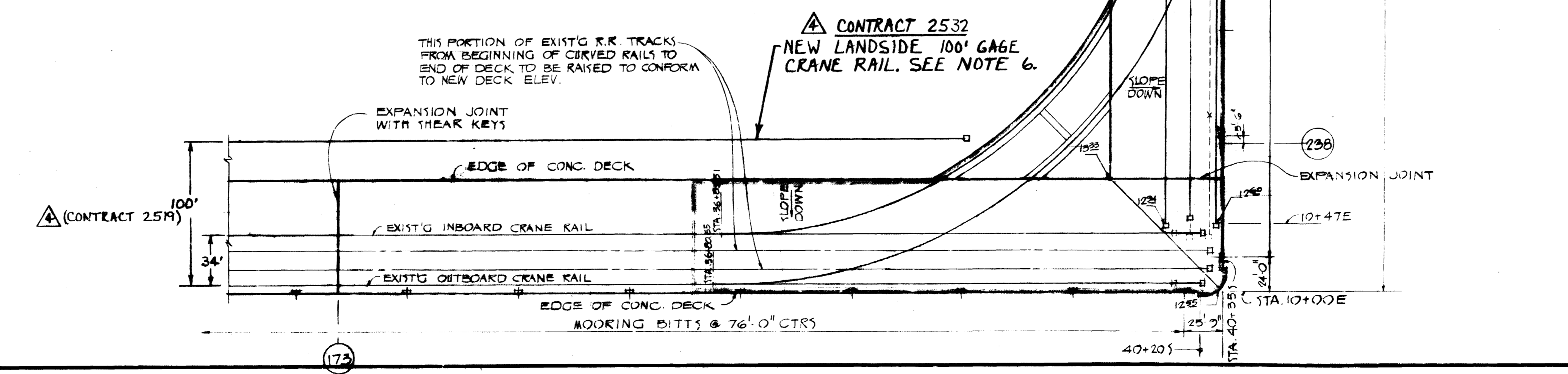
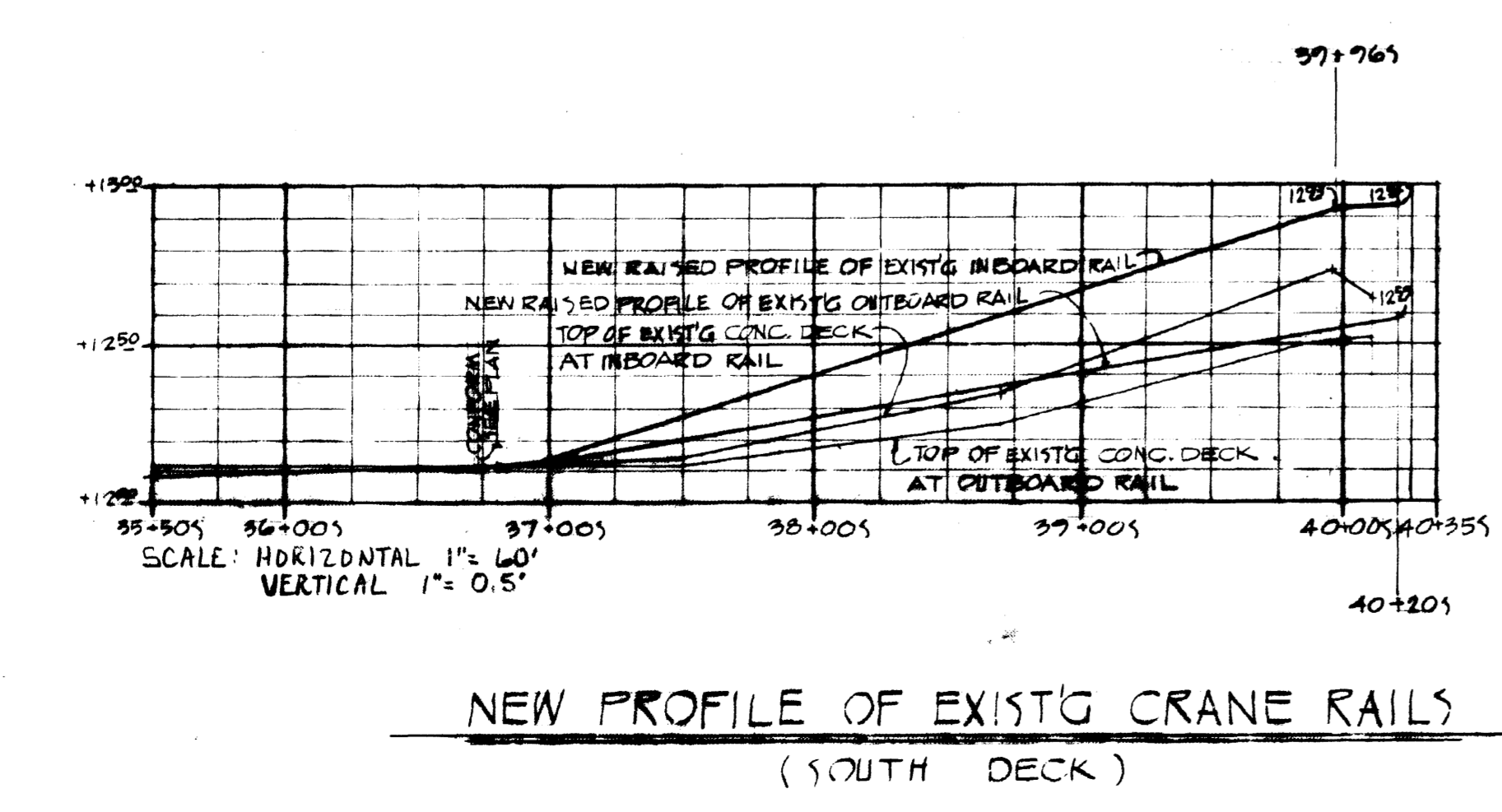
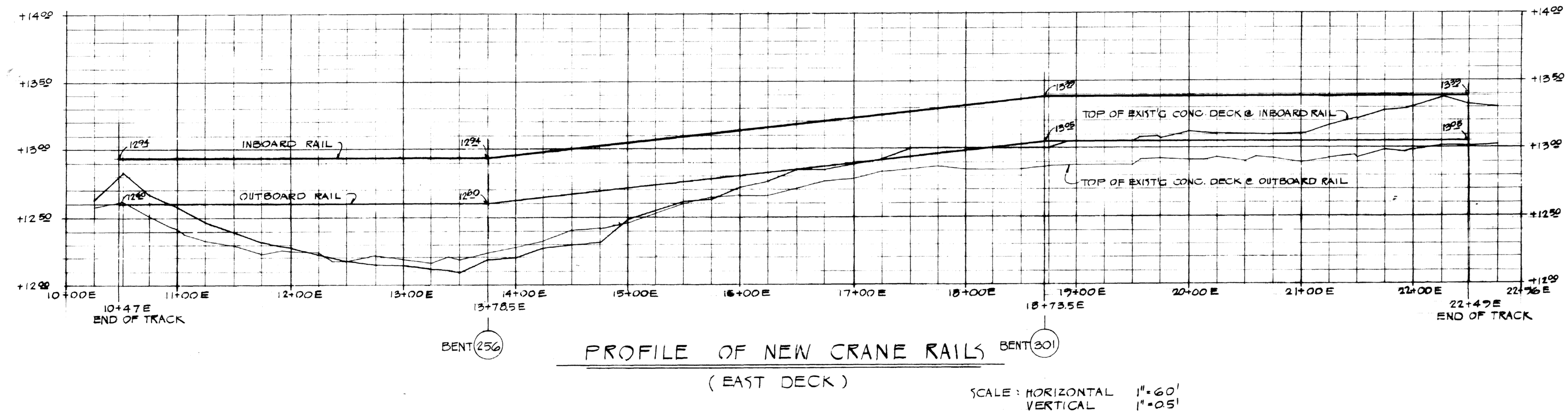
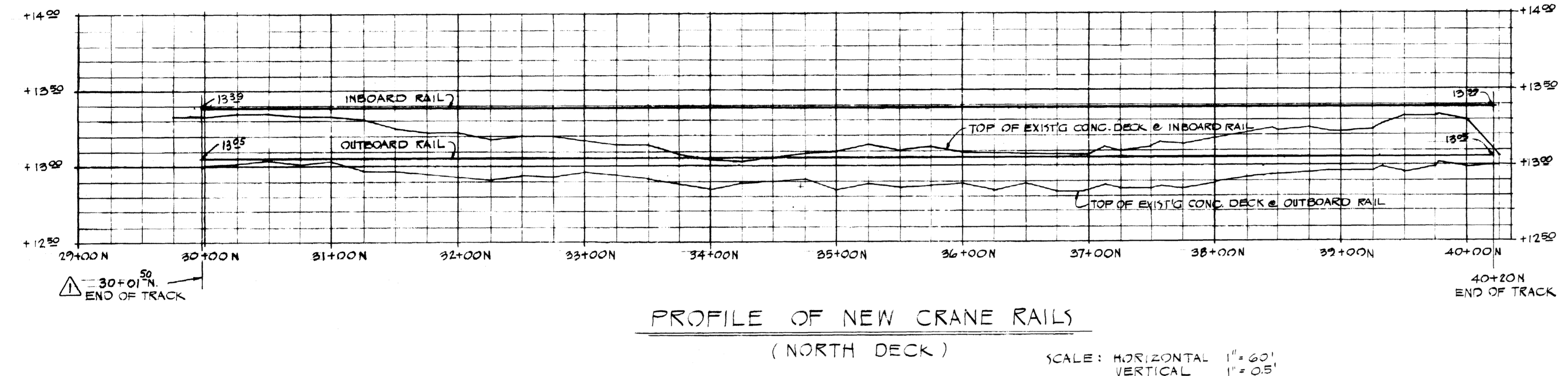


- NOTES:
- ELEVATIONS SHOWN ARE TO DATUM OF M.L.L.N.
 - CRANE RAIL DETAILED LAYOUT, SUCH AS RADIUS OF CURVES AND SPIRALS, PROGS AND TURN OUTS WILL BE FURNISHED BY CRANE MANUFACTURER.
 - LIMIT OF PAVING SHALL BE AS INDICATED ON PLAN.
 - WHERE DEMOLITION OF DECK NOT REQ'D R.R. TRACKS TO BE REMOVED MAY BE ABANDONED.
 - REUSE EXISTG MOORING BITTS.
 - FOR CONTRACT 2532 BIDDING PURPOSES - ASSUME THAT THE NEW LANDSIDE 100' GAGE CRANE RAIL ELEVATIONS WILL BE WITHIN 4" (10CM) OF THE WATER SIDE RAIL, AND THAT THE LANDSIDE RAIL WILL BE COMPLETED IN THE AREA OF SHED "A" IN TIME FOR THE ERECTION OF THE 100' GAGE CRANES.



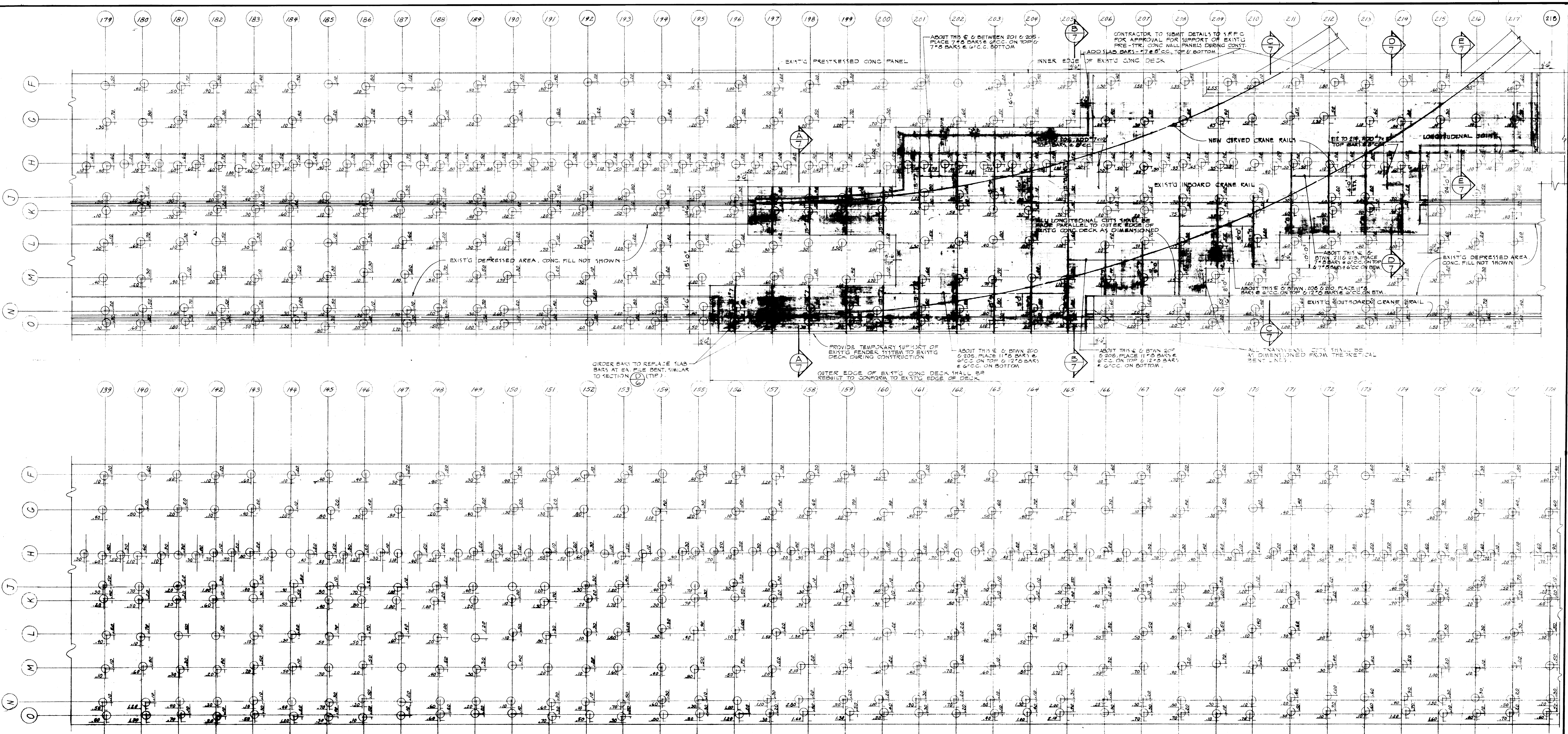
REVISIONS	
NO.	DESCRIPTION
34/1	ADDED 100' GAGE LANDSIDE CRANE RAIL TO SOUTH DECK.
32/1	STORAGE POSITION AT WEST END OF NORTH DECK LOCATED.
29/1	STORAGE POSITION OF GENTRY CRANE LOCATED.
17/15	REV. END OF RAIL AT NORTH DECK.

PORT OF SAN FRANCISCO
SAN FRANCISCO PORT COMMISSION
DEPARTMENT OF ENGINEERING

PIER 80
DECK MODIFICATION
PLAN
GENERAL DECK & CRANE RAILS

CONTRACT 2532 +
CONTRACT NO. 2422

APPROVED BY: [Signature]
MADE BY: G. MUI
CHECKED BY: P. WOOD & W.F.F.
DATE: 7-24-74
SCALE: AS NOTED
DRAWING NO.: 8581-80-1
SHEET NO.: 1
OF 14 SHEETS



FILE AND DECK PLAN
SCALE 1"=10'-0"

NOTE (A)
FILE LOCATIONS SHOWN ARE AN "AS BUILT" FILE PLAN OF ORIGINAL CONCRETE DECK. REINFORCING STEEL DESIGN IS BASED ON THESE LOCATIONS. CONTRACTOR SHALL SURVEY FILE LOCATIONS AFTER DEMOLITION OF CONCRETE DECK & SUBMIT THIS DATA TO S.F.P.C. REINF. STEEL DESIGN MAY VARY FROM CONTRACT DESIGN DUE TO FILE MOVEMENT FROM "AS BUILT" LOCATIONS.

LEGEND:

(A) SEC. MARK
(I) SHEET NO.

CONTRACT NO. 2422

APPROVED BY: [Signature]
SAN FRANCISCO PORT COMMISSION
DATE 7-24-74
DRAWING NO. 8582-80-1

REVISIONS	
NO.	DESCRIPTION

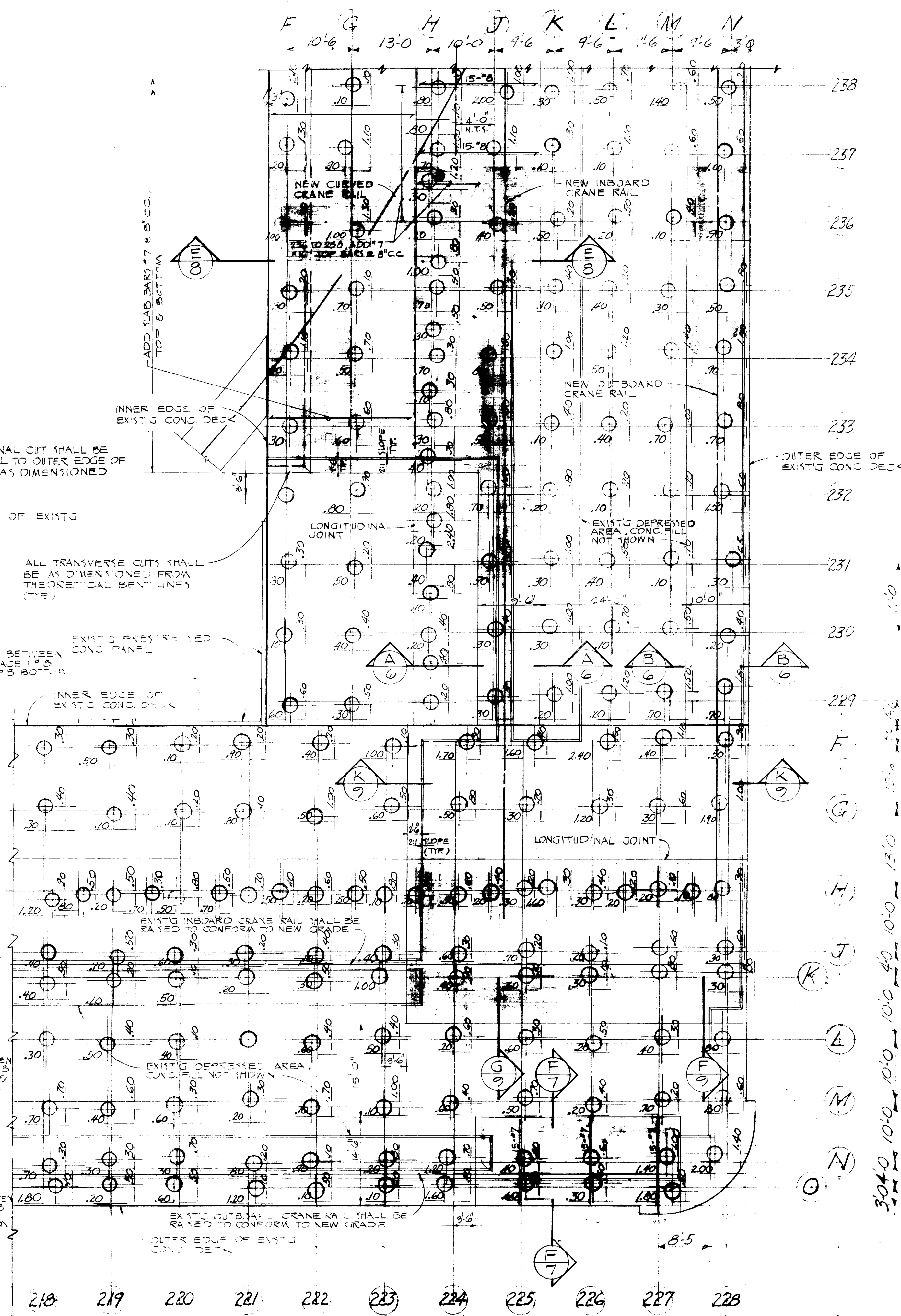
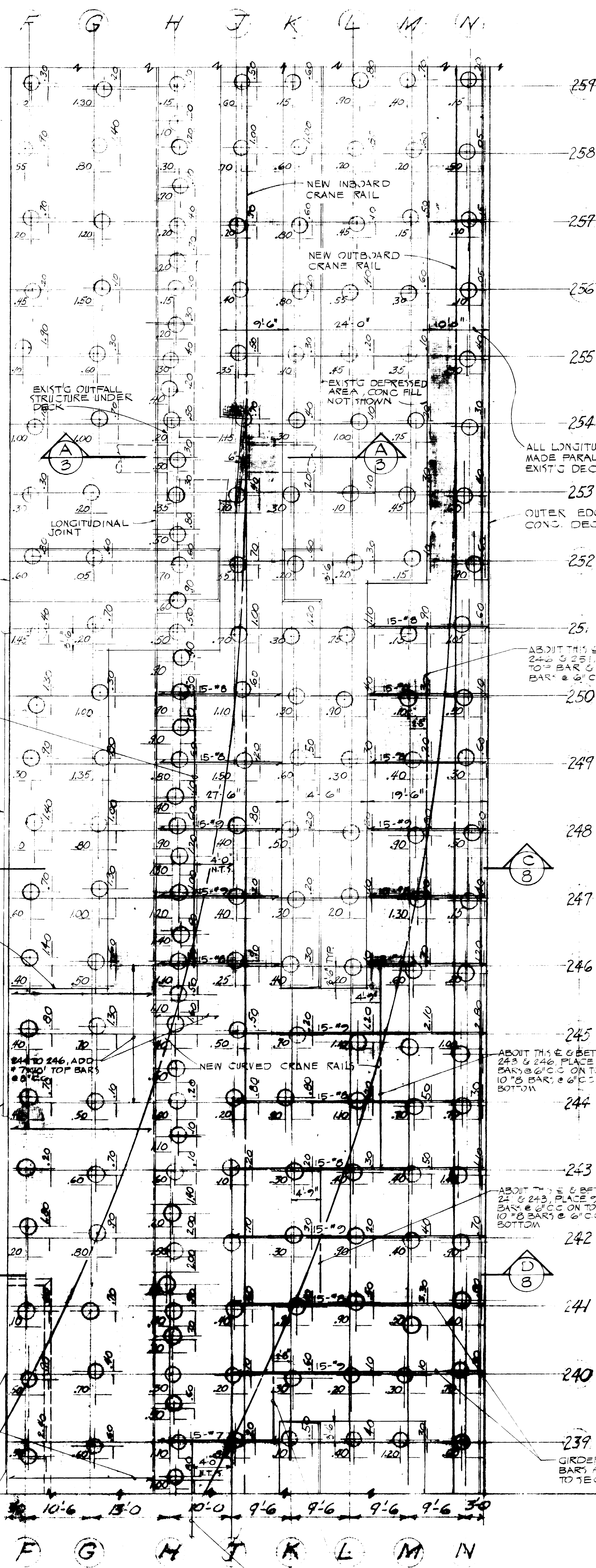
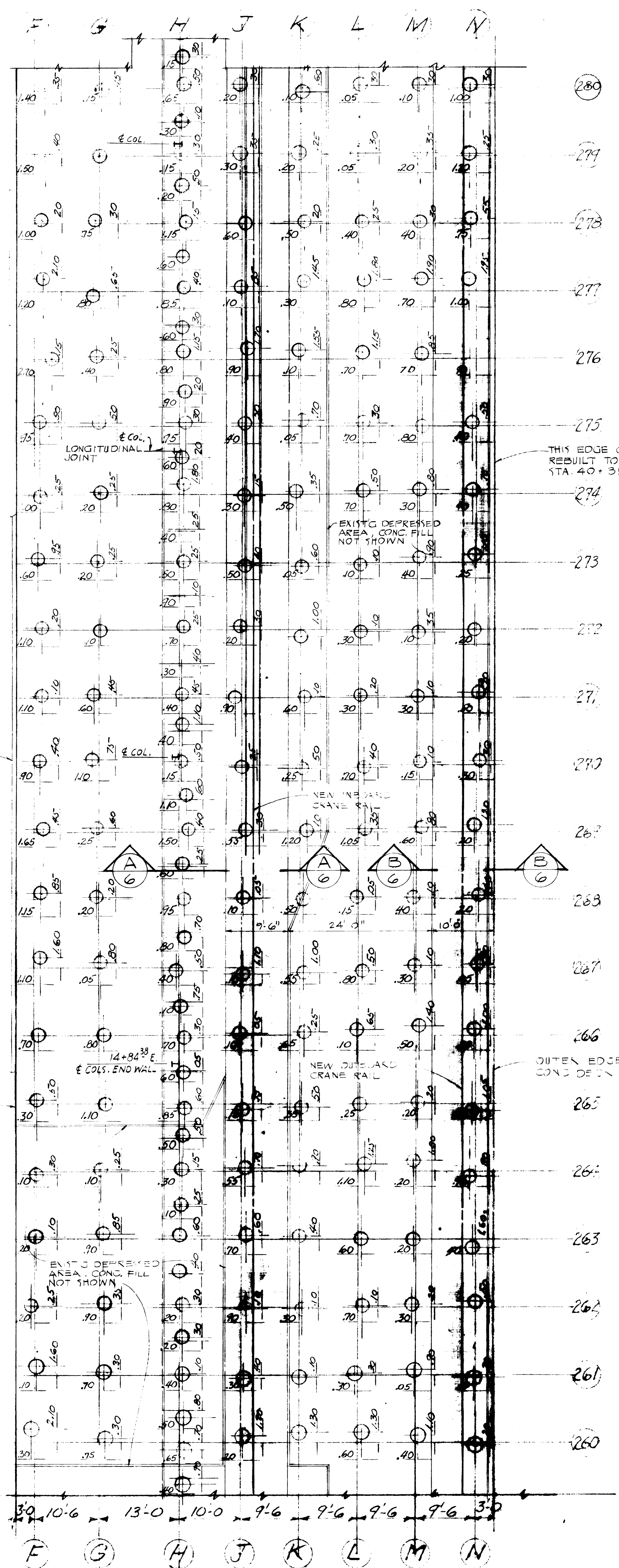
PORT OF SAN FRANCISCO
SAN FRANCISCO PORT COMMISSION
DEPARTMENT OF ENGINEERING

PIER 80
DECK MODIFICATION
FILE & DECK PLAN
SOUTH DECK

IN CHARGE OF: W. PELEAJU
MADE BY: G. HUI
TRACED BY: G. HUI
DATE: 7-24-74

CHECKED BY: P. WOOD
DATE: 7-24-74
SCALE: AS NOTED

8582-80-1



ABOUT THIS & BETWEEN 244 & 250, PLACE 7 # 8 BARS @ 6" C.C. ON TOP & 7 # 8 BARS @ 6" C.C. ON BOTTOM

EXISTING PRESTRESSED CONCRETE PANEL

ALL TRANSVERSE CUTS SHALL BE AS DIMENSIONED FROM THEORETICAL BENT LINE

CONTRACTOR TO SUBMIT DETAILS FOR SUPPORT OF EXIST'G CONCR. PANELS TO S.F.P.C. FOR APPROVAL

ALL TRANSVERSE CUTS SHALL BE AS DIMENSIONED FROM THEORETICAL BENT LINE

EXIST'G PRESTRESSED CONCR. PANEL

INNER EDGE OF EXIST'G CONCR. DECK

ABOUT THIS & BETWEEN 249 & 250, PLACE 7 # 8 BARS @ 6" C.C. ON TOP & 7 # 8 BARS @ 6" C.C. ON BOTTOM

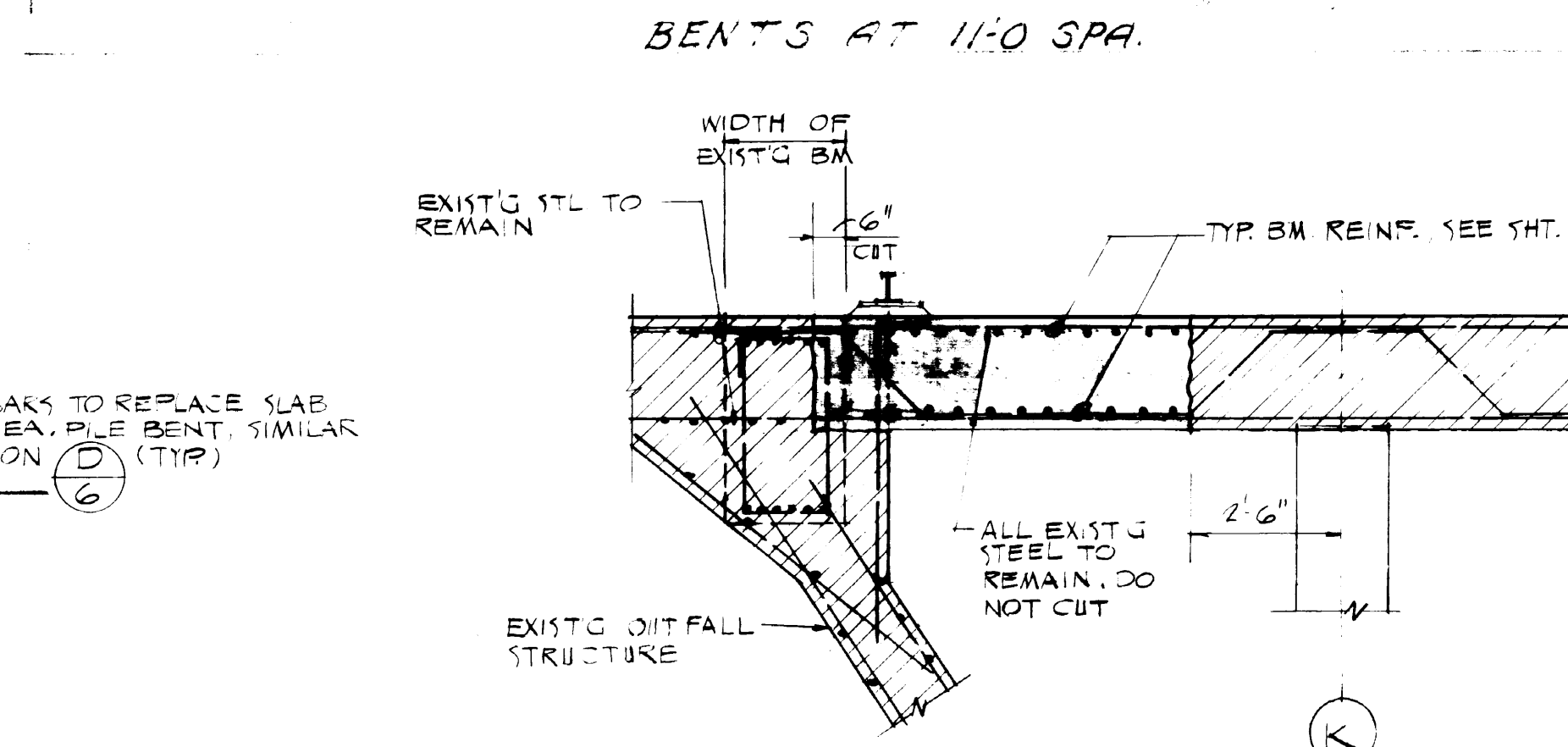
INNER EDGE OF EXIST'G CONCR. DECK

ABOUT THIS & BETWEEN 243 & 244, PLACE 7 # 8 BARS @ 6" C.C. ON TOP & 7 # 8 BARS @ 6" C.C. ON BOTTOM

ABOUT THIS & BETWEEN 242 & 243, PLACE 7 # 8 BARS @ 6" C.C. ON TOP & 7 # 8 BARS @ 6" C.C. ON BOTTOM

EXIST'G OUTBOARD CRANE RAIL SHALL BE RAISED TO CONFORM TO NEW GRADE

OUTER EDGE OF EXIST'G CONCR. DECK



PILE AND DECK PLAN

SCALE: 1" = 10'-0"

SEE NOTE (A) ON SHEET #2

SECTION (A)

SCALE: 3/8" = 1'-0"

NO.	DATE	DESCRIPTION

PORT OF SAN FRANCISCO
SAN FRANCISCO PORT COMMISSION
DEPARTMENT OF ENGINEERING

PIER 80
DECK MODIFICATION
PILE & DECK PLAN
SOUTH & EAST DECKS

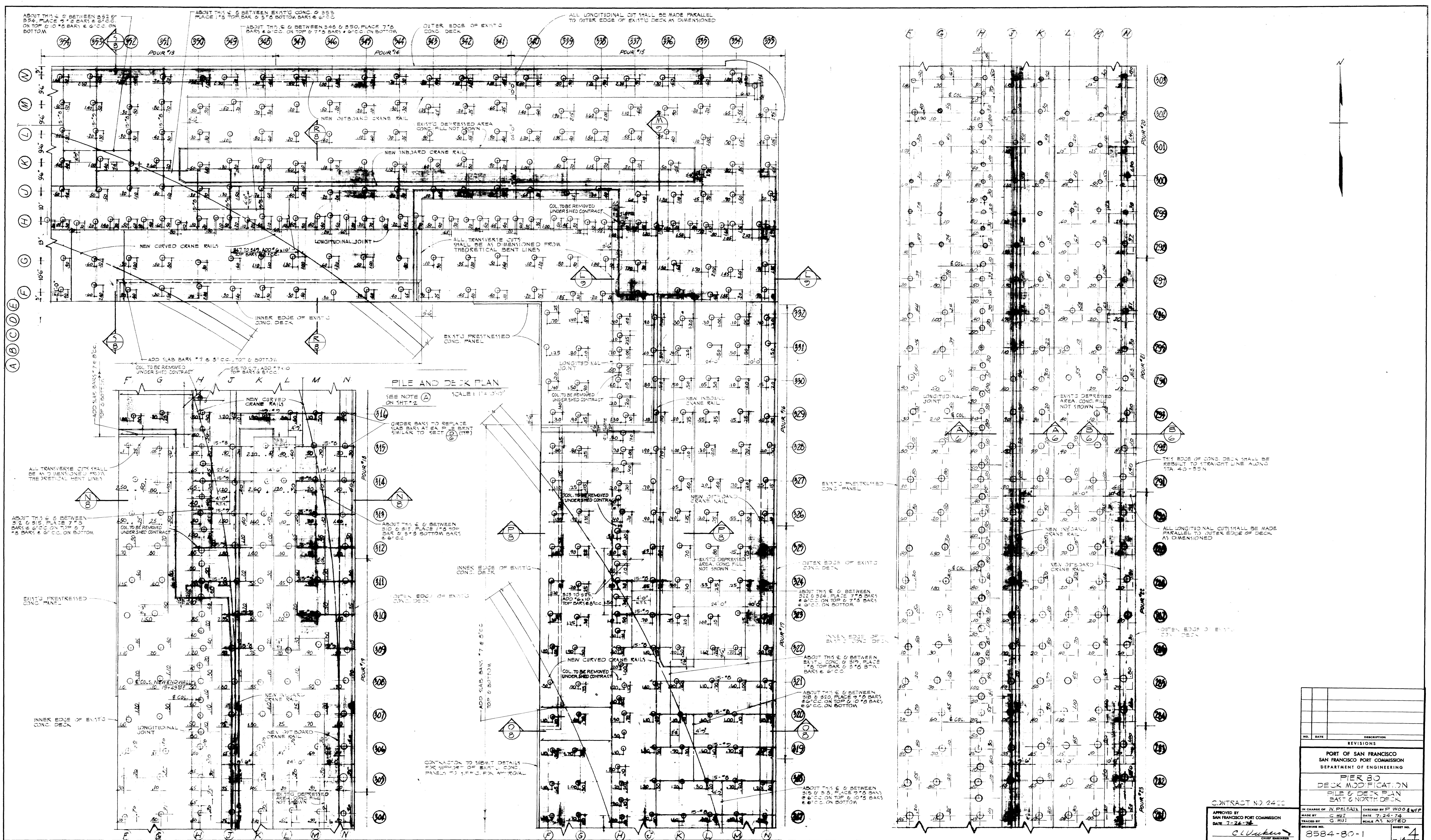
CONTRACT NO. 2422

APPROVED BY: *[Signature]*
SAN FRANCISCO PORT COMMISSION
DATE: 7-24-74

IN CHARGE OF: *[Signature]* CHECKED BY: *[Signature]*
DRAWN BY: *[Signature]* SCALE AS NOTED

8583-80-1

OF 14 SHEETS



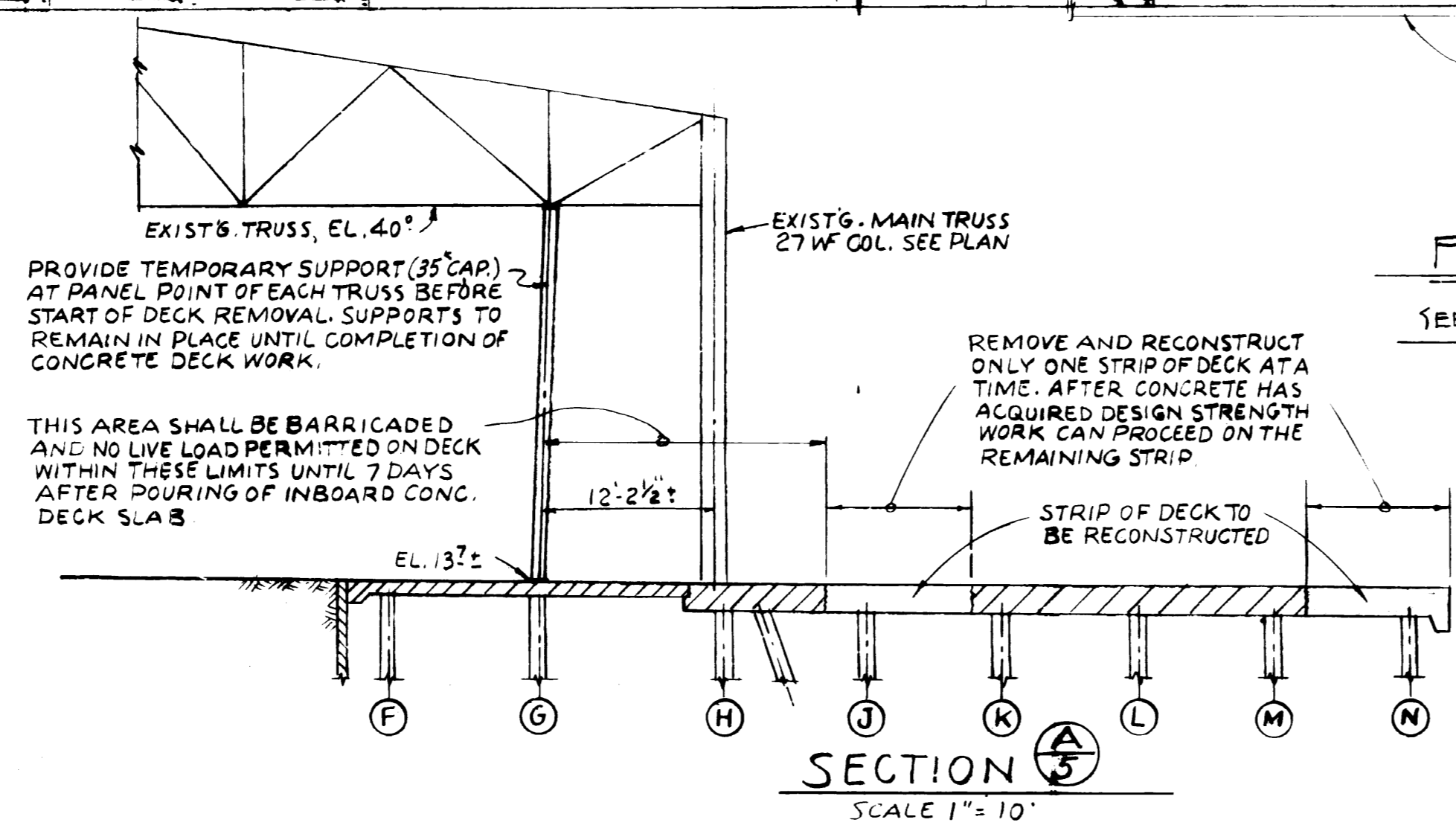
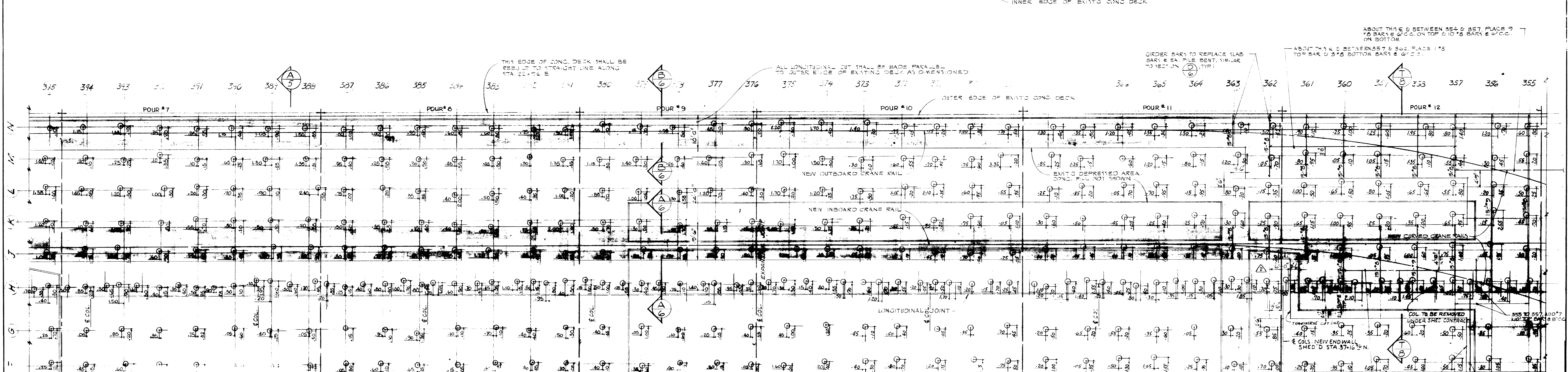
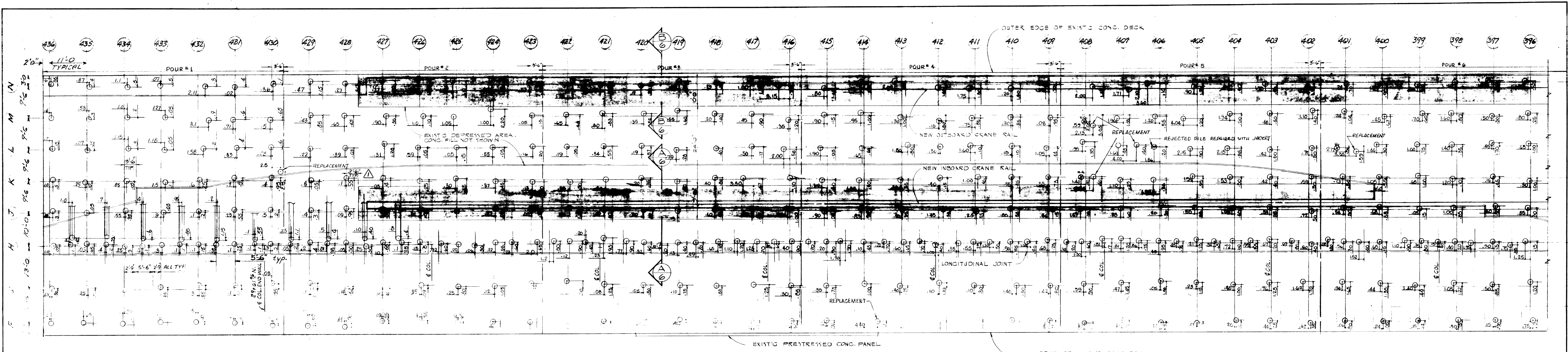
PILE AND DECK PLAN

SCALE: 1/4" = 1'-0"

NO.	DATE	DESCRIPTION
REVISIONS		
PORT OF SAN FRANCISCO SAN FRANCISCO PORT COMMISSION DEPARTMENT OF ENGINEERING		
PIER 80 DECK MODIFICATION PILE & DECK PLAN EAST & NORTH DECK		
IN CHARGE OF: N. PELEAU		CHECKED BY: P. WOOD & WEP
DESIGNED BY: G. HUI	DATE: 7-24-74	TRACED BY: G. HUI
DRAWING NO. 8584-80-1		SHEET NO. 4

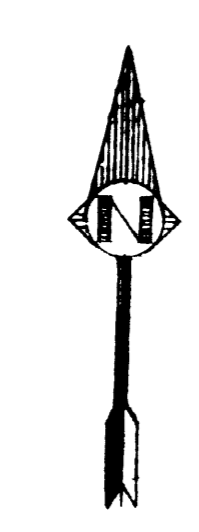
CONTRACT NO 2400
APPROVED BY: [Signature]
SAN FRANCISCO PORT COMMISSION
DATE: 7-24-74





FILE AND DECK PLAN
SEE NOTE (A) ON SHT. #2 SCALE: 1" = 10'-0"

SAMPLE: PILE IS 0.5 FEET EAST & 0.7 FEET NORTH OF INTENDED LOCATION.

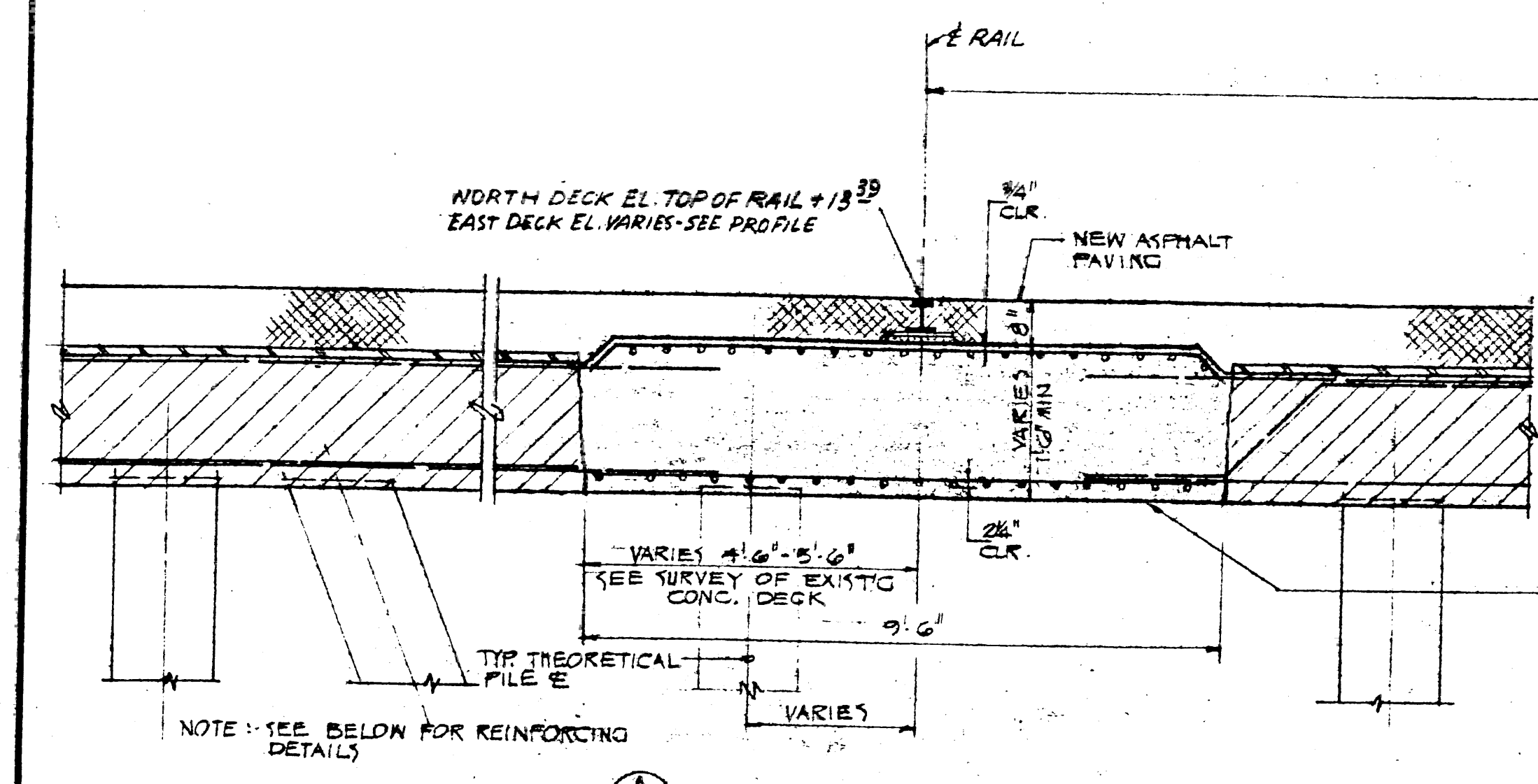


REVISIONS	
NO.	DESCRIPTION
1	REVISED TRANSVERSE CUT LINE
2	REV. END OF RAIL AT NORTH DECK

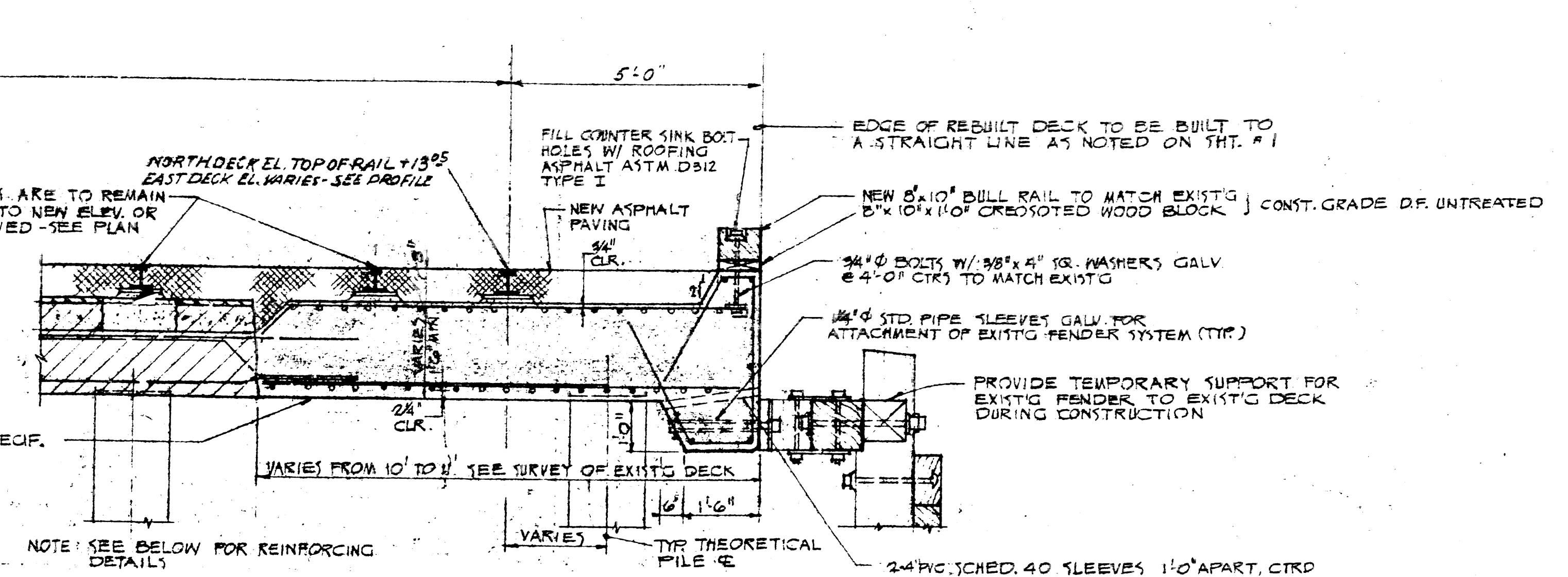
CONTRACT NO. 2422
 APPROVED BY: [Signature] DATE: 7-24-74
 MADE BY: G. HUI
 CHECKED BY: P. WOODRUFF
 TRACED BY: G. HUI
 SCALE: AS NOTED
 SHEET NO. 5 OF 14 SHEETS
 8585-80-1

TABLE FOR GIRDER REINFORCEMENT: *Continued from*

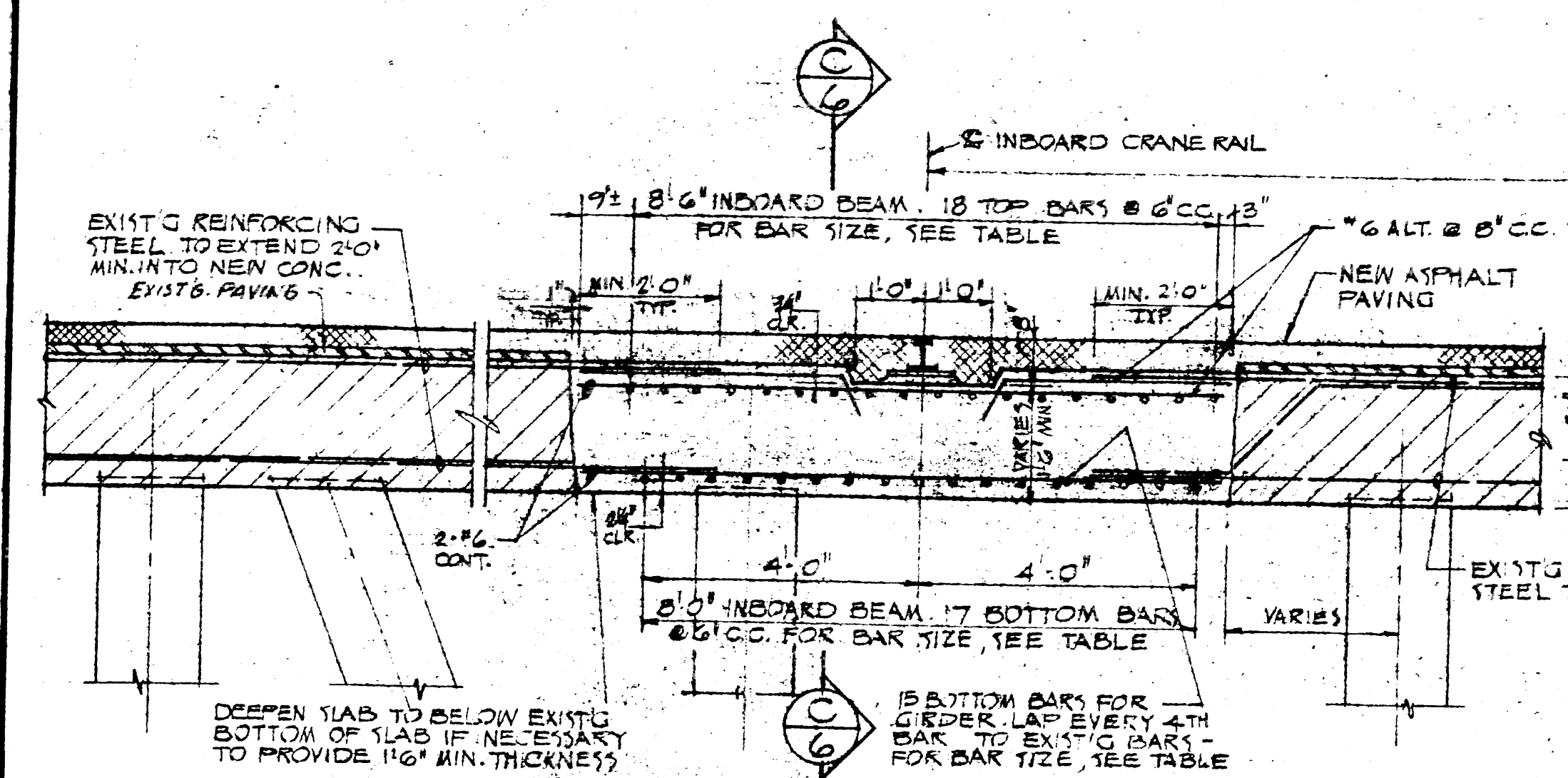
GIRDER REINFORCING TO BE 15 #7 BARS EXCEPT FOR GIRDERS AT BENTS LISTED BELOW AND AT LOCATIONS SHOWN ON PLAN					
GIRDERS FOR OUTBOARD BEAM			GIRDERS FOR INBOARD BEAM		
15 #8 BARS	15 #9 BARS	15 #10 BARS	15 #8 BARS	15 #9 BARS	15 #10 BARS
268 373 430	252	417	231 370	351	334
270 374	397	422	233 371	284	336
271 375	413	424	246 380	271	337
254 377	419	425	272 402	333	
287 380	423	426	277 411	325	
288 382	428	427	279 415	329	
293 386	433	429	281 418	331	
300 414			284 425		
325 418			346		
334 430			369		



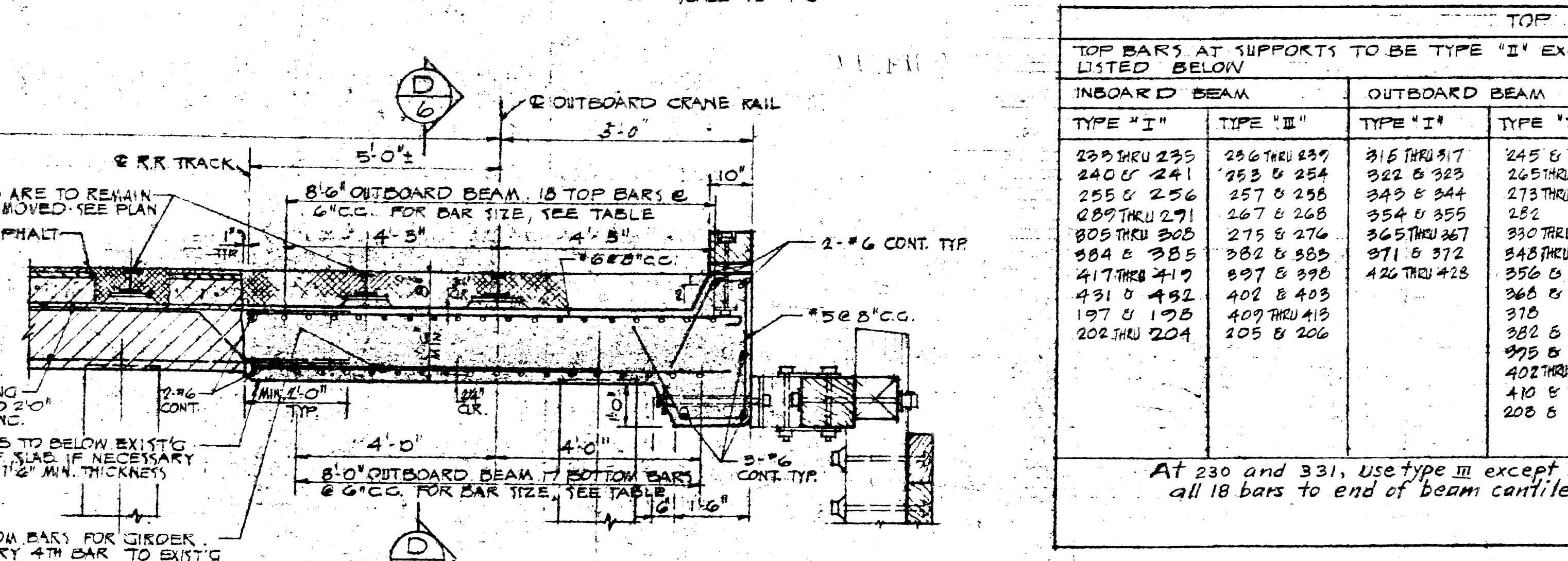
SECTION (A) - GIRDERS FOR INBOARD BEAM
(WHERE TOP OF RAIL IS 12" APPROX. ABOVE TOP OF EXISTING DECK SLAB)
SCALE: 1/2"=1'-0"



SECTION (B) - GIRDERS FOR OUTBOARD BEAM
(WHERE TOP OF RAIL IS 15" APPROX. ABOVE TOP OF EXISTING DECK SLAB)
SCALE: 1/2"=1'-0"



SECTION (A) - GIRDERS FOR INBOARD BEAM
(WHERE TOP OF RAIL IS ABOUT THE SAME ELEV. AS TOP OF EXISTING DECK SLAB)
SCALE: 1/2"=1'-0"

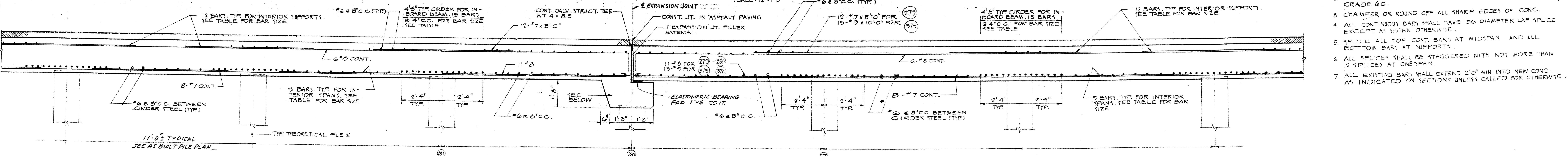


SECTION (B) - GIRDERS FOR OUTBOARD BEAM
(WHERE TOP OF RAIL IS ABOUT THE SAME ELEV. AS TOP OF EXISTING DECK SLAB)
SCALE: 1/2"=1'-0"

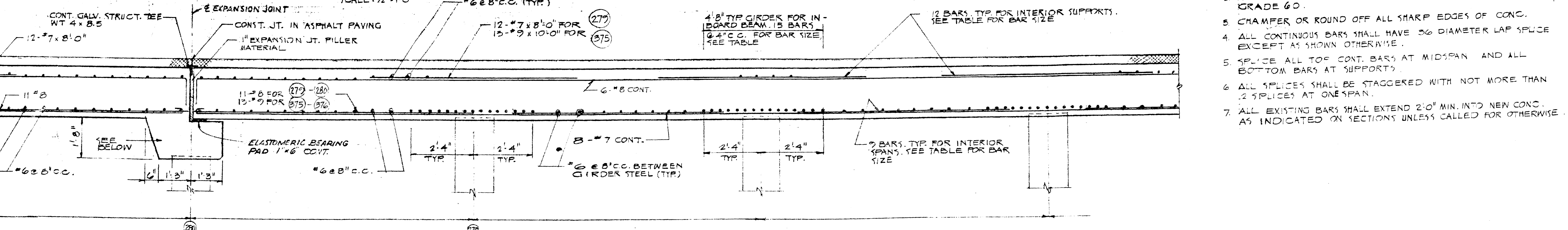
TABLE FOR CRANE BEAM REINFORCEMENT:

TOP BARS					BOTTOM BARS				
TOP BARS AT SUPPORTS TO BE TYPE "I" EXCEPT AT BENTS LISTED BELOW					BOTTOM BARS BETWEEN SUPPORTS TO BE TYPE "C" EXCEPT AT SPANS LISTED BELOW				
INBOARD BEAM		OUTBOARD BEAM			INBOARD BEAM		OUTBOARD BEAM		
TYPE "I"	TYPE "II"	TYPE "I"	TYPE "II"	TYPE "III"	TYPE "B"	TYPE "D"	TYPE "B"	TYPE "D"	REMARKS:
230 THRU 235	236 THRU 239	315 THRU 317	345 & 346	233 & 234	283 - 235	256 - 259	228 - 242	291 - 235	TYPE "B" 15 #7 CONT. 15 #6 x 7'0" OR CC OF SUPPORTS
240 & 241	250 & 254	322 & 323	265 THRU 267	356 & 359	249 - 252	274 - 275	252 - 260	244 - 247	TYPE "C" 15 #7 CONT. 15 #7, 8" OR CC OF SUPPORTS
255 & 256	257 & 258	349 & 344	273 THRU 275	365 & 364	249 - 252	274 - 275	292 - 294	264 - 270	TYPE "D" 15 #7 CONT. 15 #7, 8" OR CC OF SUPPORTS
289 THRU 291	267 & 268	354 & 355	282	375 & 374	277 - 279	405 - 411	324 - 319	272 - 278	TYPE "D" 15 #7 CONT. 15 #7, 8" OR CC OF SUPPORTS
303 THRU 305	275 & 276	365 THRU 367	330 THRU 340	409 THRU 409	285 - 310	229 - 230	341 - 344	355 - 360	TYPE "D" 15 #7 CONT. 15 #7, 8" OR CC OF SUPPORTS
304 & 305	302 & 303	371 & 372	346 THRU 352	412 THRU 416	313 - 327	331 - 332	345 - 349	365 - 367	
417 THRU 419	397 & 398	407 & 408	356 & 357	422 & 425	369 - 369		369 - 369	367 - 370	
431 & 432	402 & 403	426 THRU 423	368 & 367	452 & 433	415 - 420		379 - 381	371 - 375	
197 & 198	407 THRU 413		370	200 & 201	421 - 425		389 - 391	421 - 424	
202 THRU 204	205 & 206		372 & 363	225 & 226	431 - 435		397 - 401	431 - 434	
			403 THRU 404	403 THRU 404	431 - 435		401 - 405	199 - 205	
			410 & 411	208 & 204	209 - 213		426 - 431	229 - 230	
			208 & 204					331 - 332	

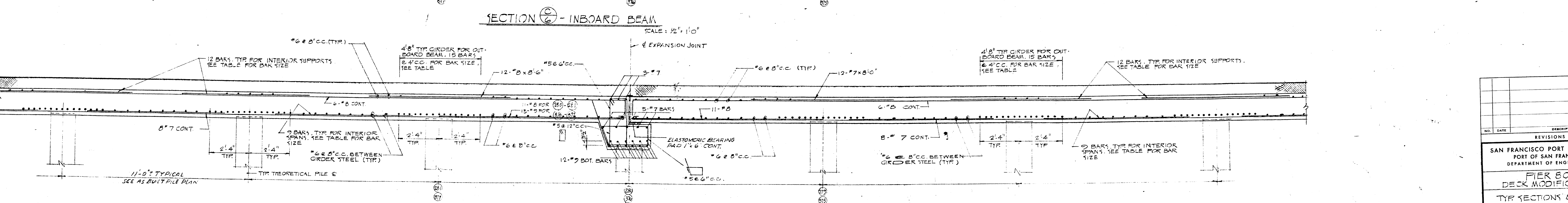
At 230 and 331, use type m except extending all 18 bars to end of beam cantilever.



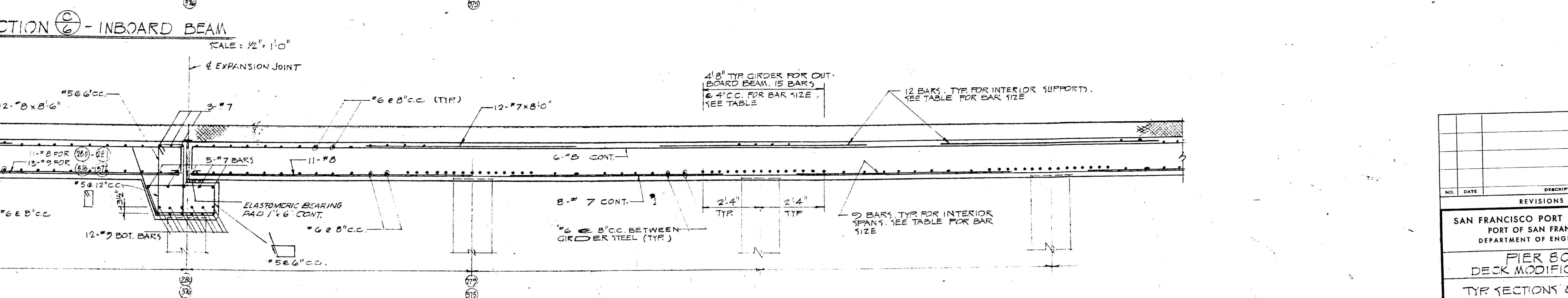
SECTION (A) - GIRDERS FOR INBOARD BEAM
SCALE: 1/2"=1'-0"



SECTION (B) - GIRDERS FOR OUTBOARD BEAM
SCALE: 1/2"=1'-0"



SECTION (C) - INBOARD BEAM
SCALE: 1/2"=1'-0"



SECTION (D) - OUTBOARD BEAM
SCALE: 1/2"=1'-0"

- NOTES:
- CONC. SHALL ATTAIN A COMPRESSIVE STRENGTH OF 3000 P.S.I. AT 28 DAYS.
 - REINFORCING STEEL SHALL CONFORM TO ASTM A618-71 GRADE 60.
 - CRAMPER OR ROUND OFF ALL SHARP EDGES OF CONC.
 - ALL CONTINUOUS BARS SHALL HAVE 2x DIAMETER LAP SPICE EXCEPT AS SHOWN OTHERWISE.
 - SPICE ALL TOP CONT. BARS AT MIDSPAN AND ALL BOTTOM BARS AT SUPPORTS.
 - ALL SPLICES SHALL BE STAGGERED WITH NOT MORE THAN 2 SPLICES AT ONE SPAN.
 - ALL EXISTING BARS SHALL EXTEND 2'-0" MIN. INTO NEW CONC. AS INDICATED IN SECTIONS UNLESS CALLED FOR OTHERWISE.

CONTRACT NO. 2422

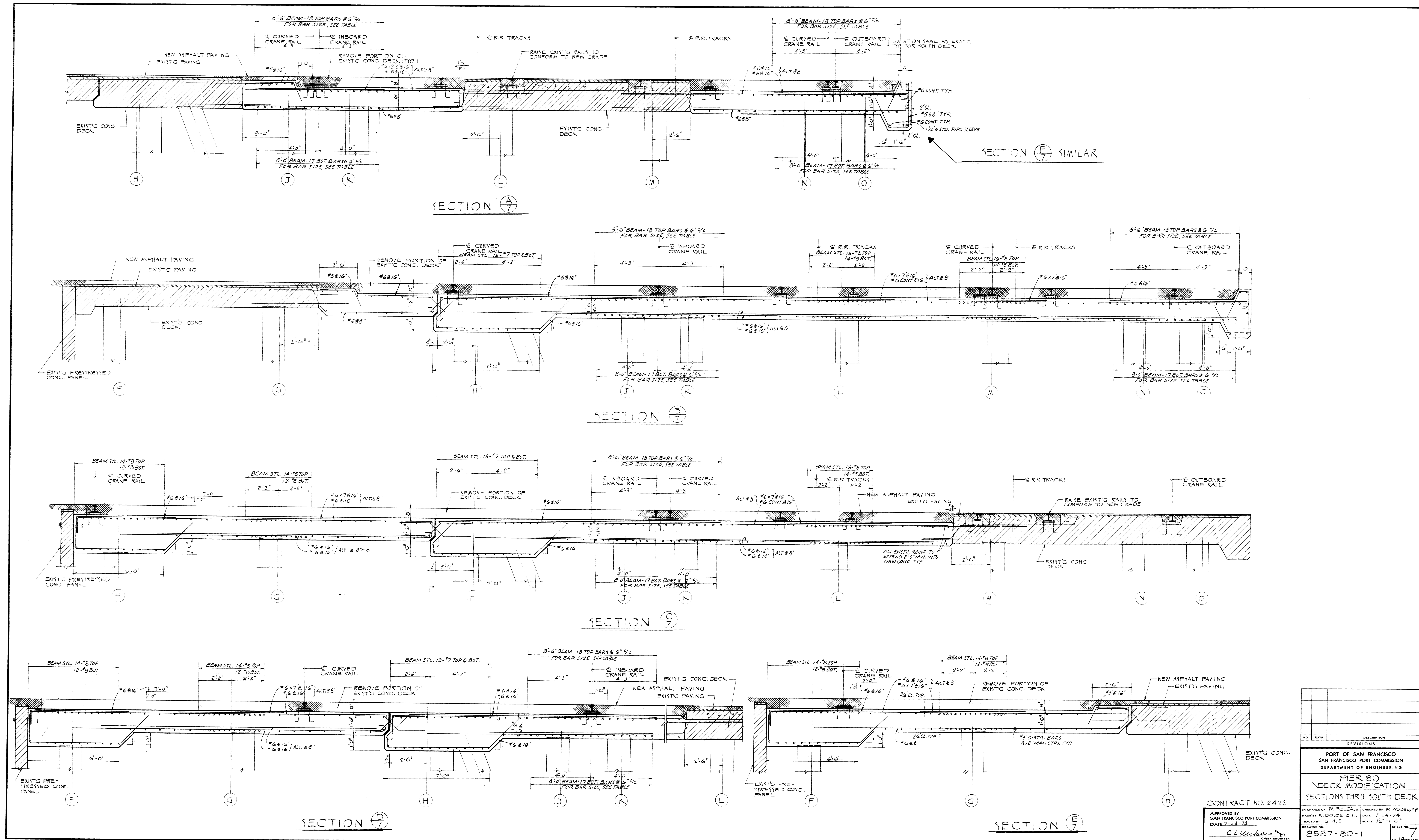
APPROVED BY: *CLV*
DATE: 7-24-74

IN CHARGE OF: W. PELLEUX
MADE BY: K. BOLES CH
TRACED BY: G. HUI
CHECKED BY: P. WOODRUFF
DATE: 7-24-74
SCALE: AS NOTED

SAN FRANCISCO PORT COMMISSION
PORT OF SAN FRANCISCO
DEPARTMENT OF ENGINEERING

PIER 80
DECK MODIFICATION
TYP. SECTIONS & DETAILS

8586-80-1

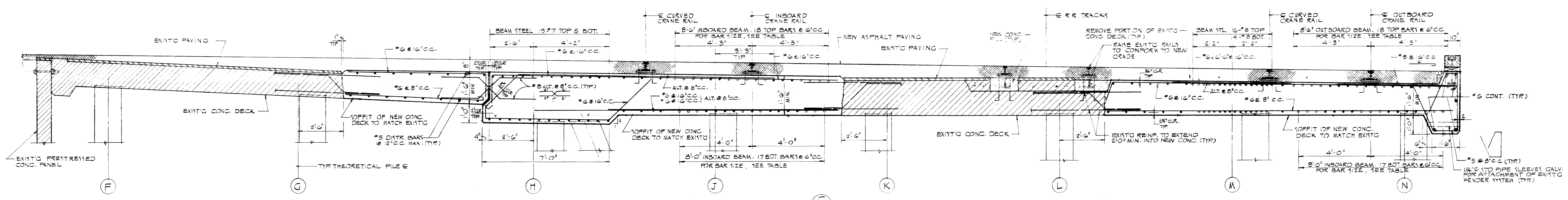


NO.	DATE	DESCRIPTION

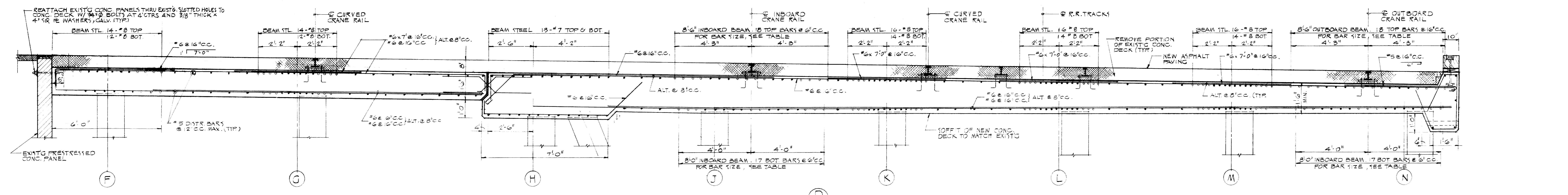
PORT OF SAN FRANCISCO
 SAN FRANCISCO PORT COMMISSION
 DEPARTMENT OF ENGINEERING
PIER 80
DECK MODIFICATION
 SECTIONS THRU SOUTH DECK

IN CHARGE OF: **W. PELEAK** CHECKED BY: **F. WOODRUP**
 MADE BY: **K. BOLCE C.H.** DATE: **7-24-74**
 DRAWING NO.: **G. HUI** SCALE: **1/2" = 1'-0"**
 DRAWING NO.: **8587-80-1** SHEET NO.: **7**
 OF 14 SHEETS

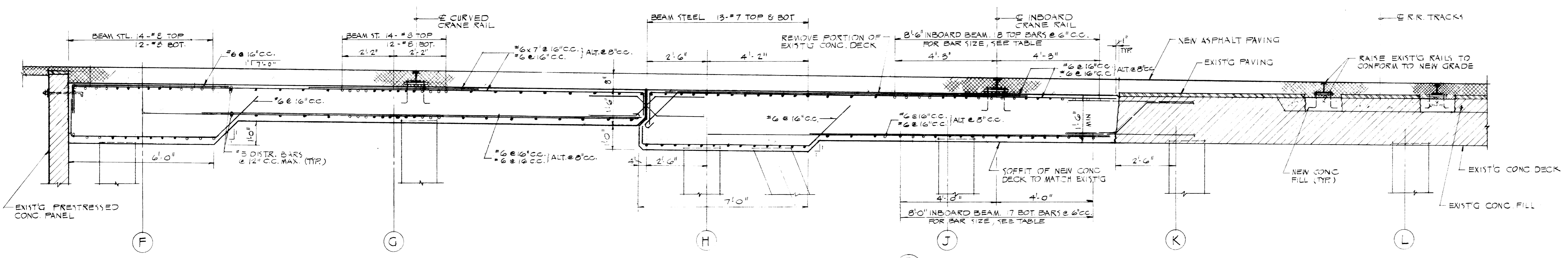
CONTRACT NO. 2422
 APPROVED BY:
 SAN FRANCISCO PORT COMMISSION
 DATE 7-24-74
C. L. Vickers
 CHIEF ENGINEER



SECTION C
SECTIONS N & T SIMILAR

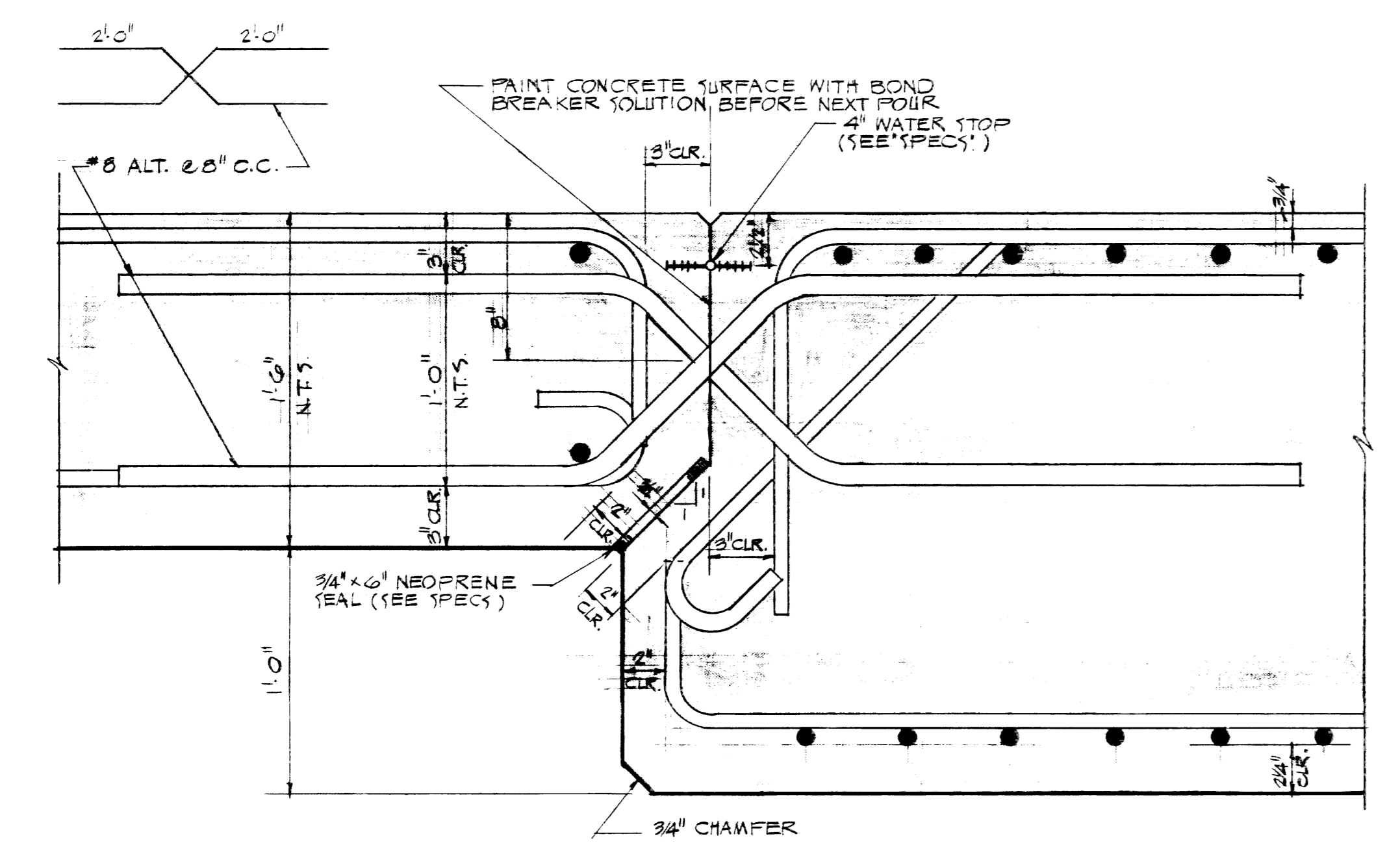


SECTION D
SECTIONS Q & S SIMILAR



SECTION E
SECTIONS P & R SIMILAR

NOTES ON STEEL PLACEMENT FOR SHEETS 7, 8 AND 9:
 FOR REINFORCING STEEL NOT SHOWN ON SECTIONS ON SHEETS 7, 8 & 9 SEE ADDITIONAL REIN. ON PLAN SHEETS 2, 3, 4 & 5.
 BEAM STEEL: MOVE ONE SHORT TOP & BOTTOM BEAM BARS FOR EVERY 6" THAT PILE IS OFF DESIGNED LOCATION TRANSVERSELY, FROM ONE SIDE TO THE SIDE WHERE PILE IS OFF CENTER.
 CENTER ALL SHORT TOP BEAM BARS OVER ACTUAL DRIVEN PILE AND CENTER ALL BOTTOM SHORT BARS BETWEEN TWO PILES.
 SLAB STEEL: CENTER ALL SHORT TOP SLAB BARS OVER ACTUAL DRIVEN PILE.
 LEGEND:
 ○ TOP SHORT BEAM BAR
 ○ BOTTOM SHORT BEAM BAR
 ● CONT. TOP AND BOTTOM BARS

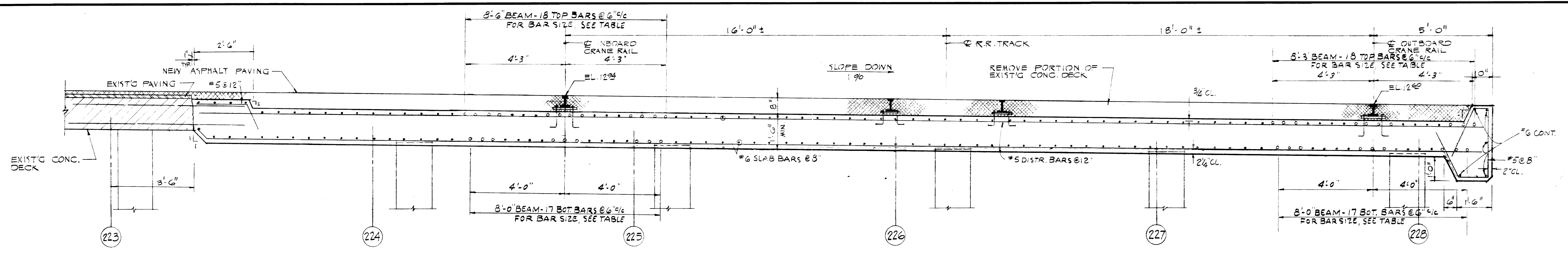


SECTION AT LONGITUDINAL JOINT
SCALE: 2"=1'-0"

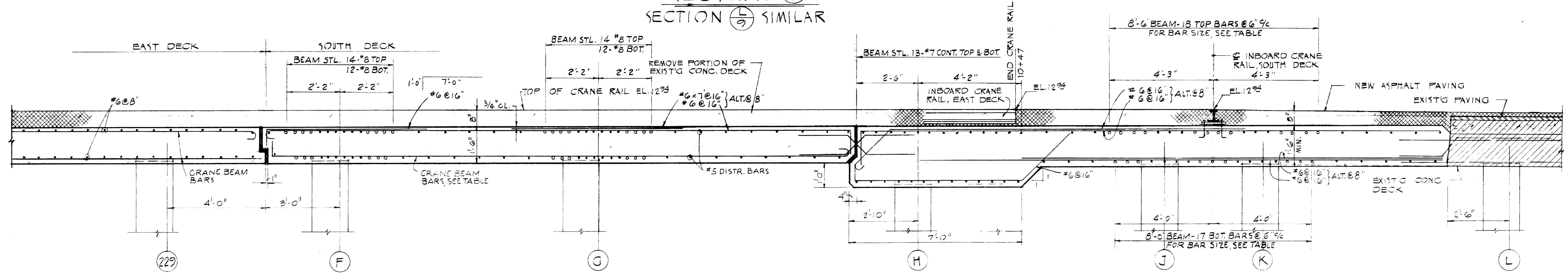
NO.	DATE	REVISIONS

PORT OF SAN FRANCISCO
 SAN FRANCISCO PORT COMMISSION
 DEPARTMENT OF ENGINEERING
PIER 80 DECK MODIFICATION
 SECTIONS THRU E & N DECKS

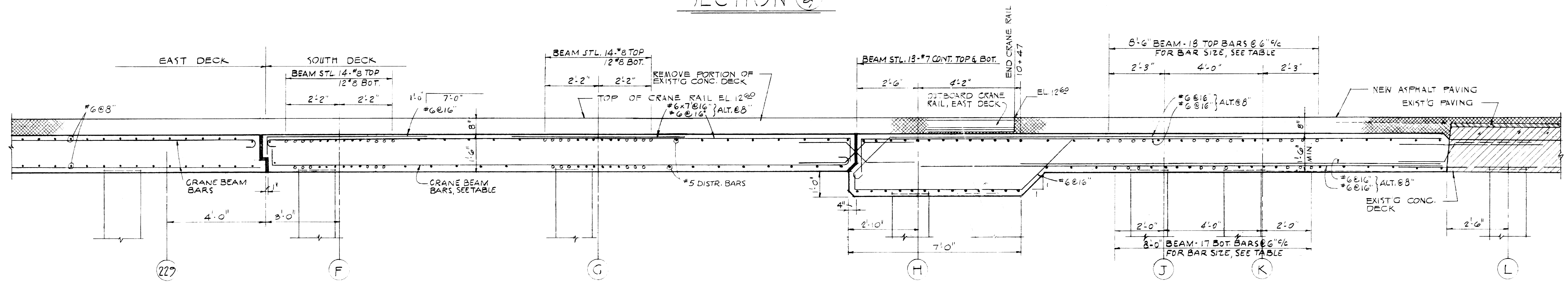
CONTRACT NO. 2422
 APPROVED BY: [Signature] CHECKED BY: P. WOOD
 MADE BY: K. BOLCE G.H. DATE: 7-24-74
 TRACED BY: G. RUIZ SCALE: 1/4"=1'-0"
 DATE: 7-24-74
 DRAWING NO. 8588-80-1 SHEET NO. 8 OF 14 SHEETS



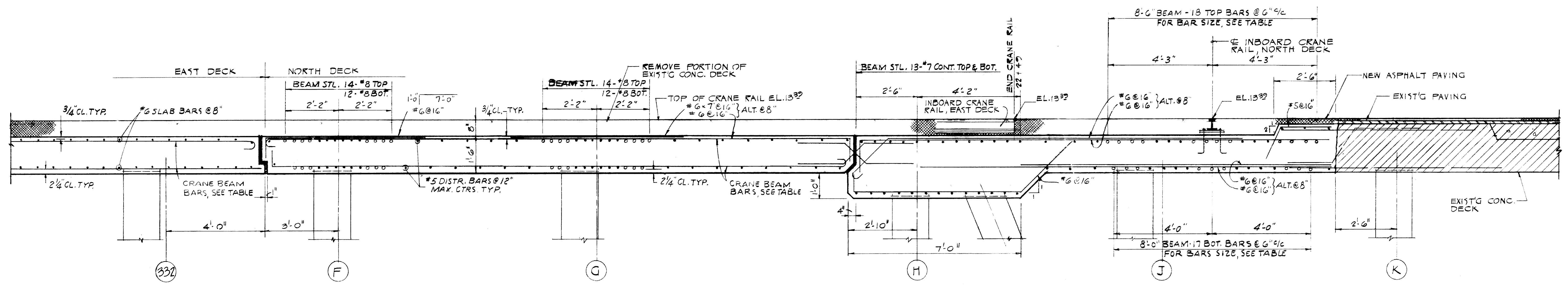
SECTION (K)
SECTION (S) SIMILAR



SECTION (S)

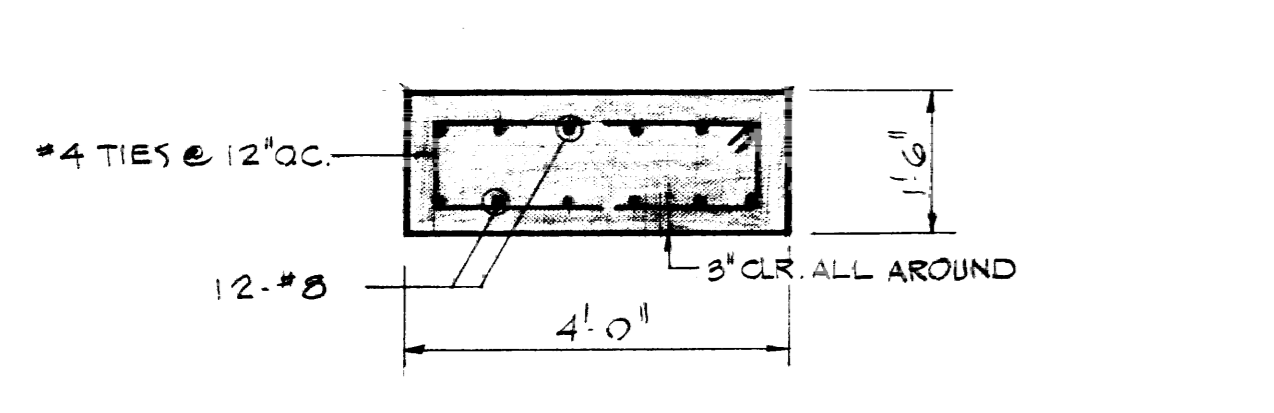


SECTION (S)

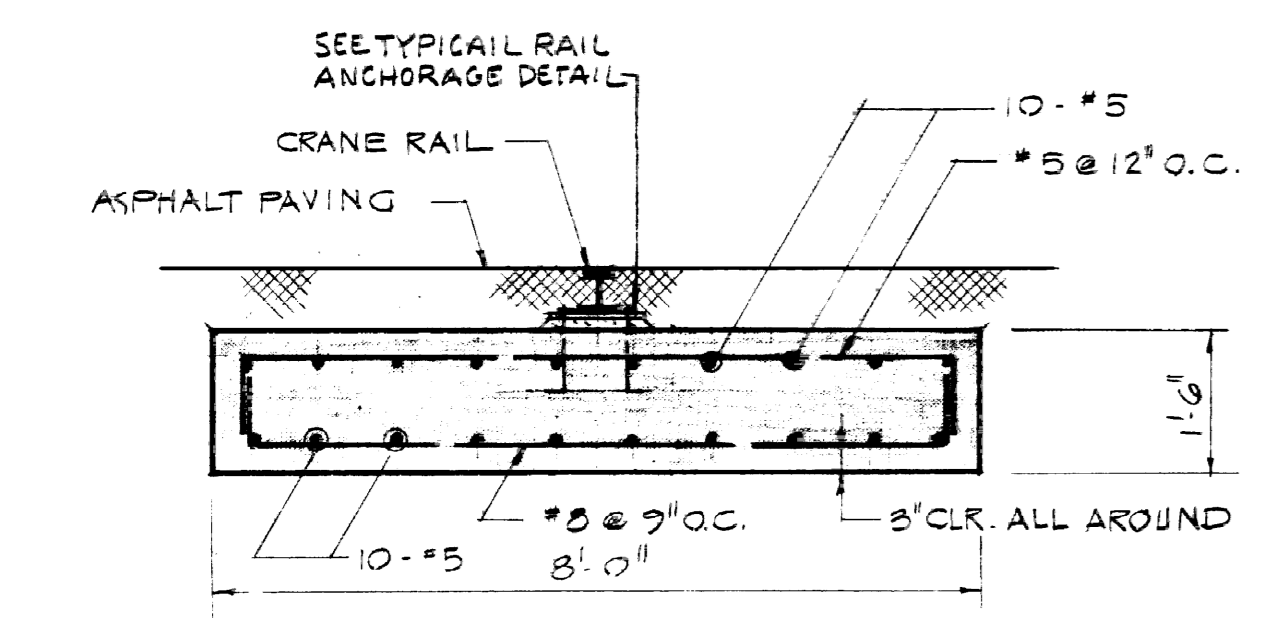


SECTION (S)

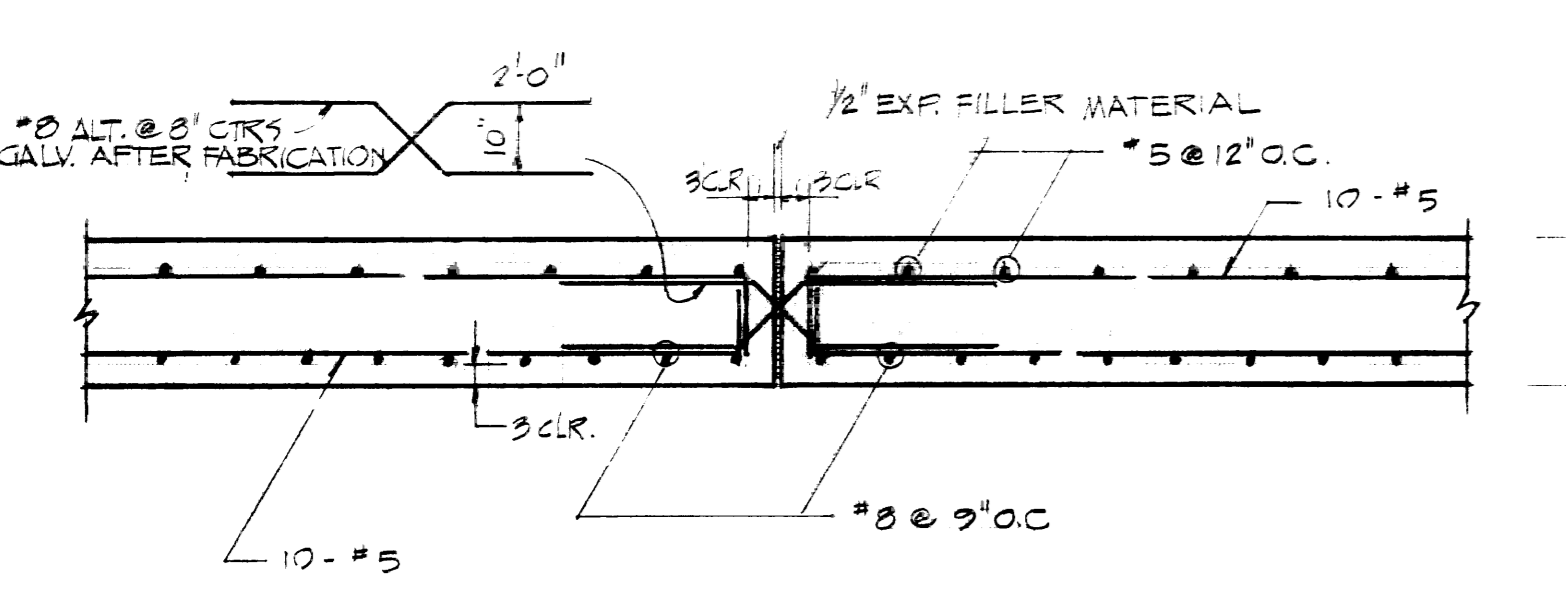
NO.	DATE	DESCRIPTION
REVISIONS		
PORT OF SAN FRANCISCO SAN FRANCISCO PORT COMMISSION DEPARTMENT OF ENGINEERING		
PIER 80 DECK MODIFICATION		
SECTIONS THRU E. & N. DECKS		
CONTRACT NO. 2422		IN CHARGE OF W. PELEAUX
APPROVED BY SAN FRANCISCO PORT COMMISSION DATE 7-24-74		CHECKED BY R. WOOD
DRAWN BY C. HUI		SCALE 1/8" = 1'-0"
DRAWING NO. 8589-80-1		SHEET NO. 3
C. L. WILSON CHIEF ENGINEER		OF 16 SHEETS



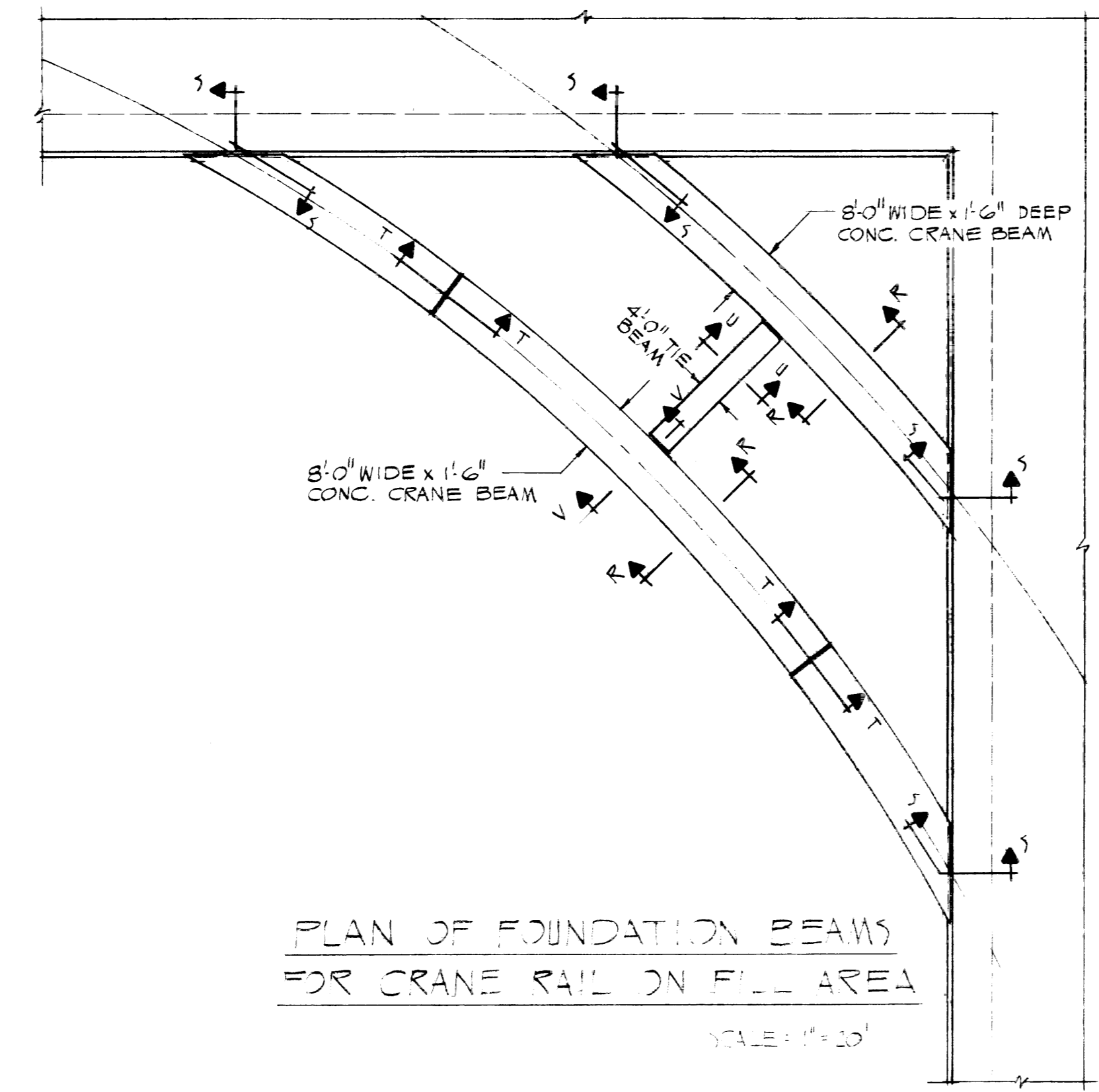
SECTION U-U
SCALE: 1/2" = 1'-0"



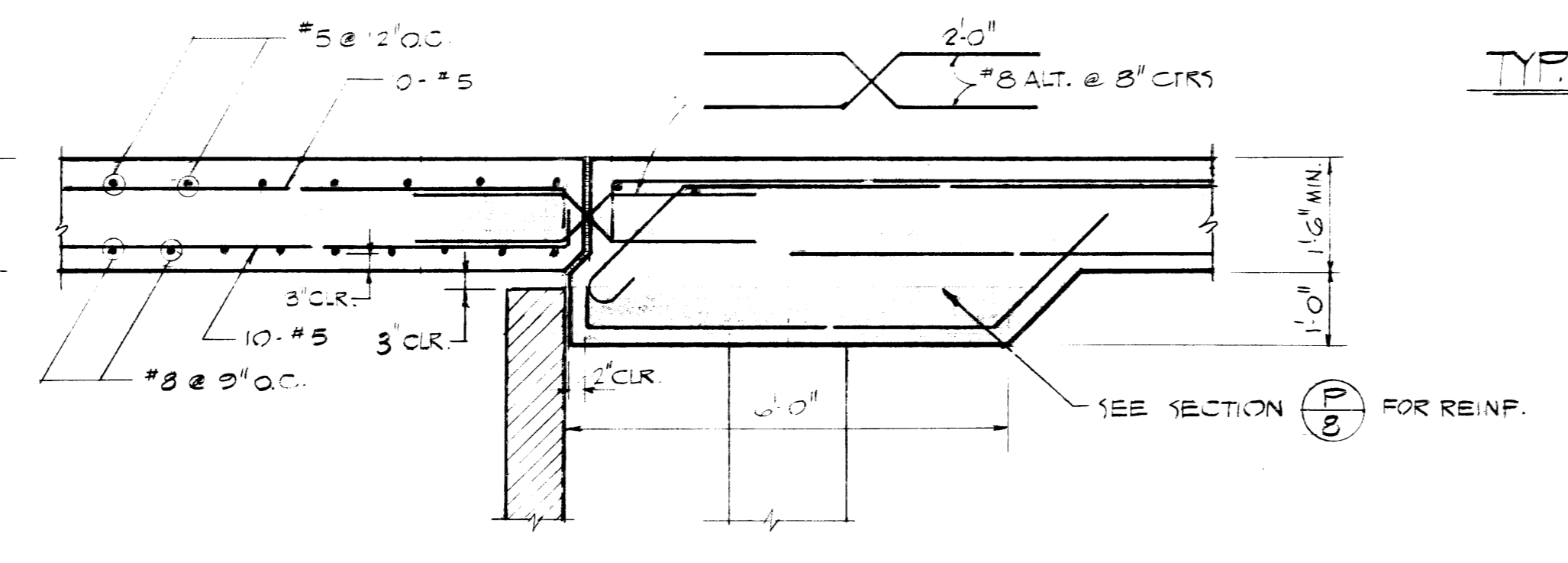
SECTION R-R
SCALE: 1/2" = 1'-0"



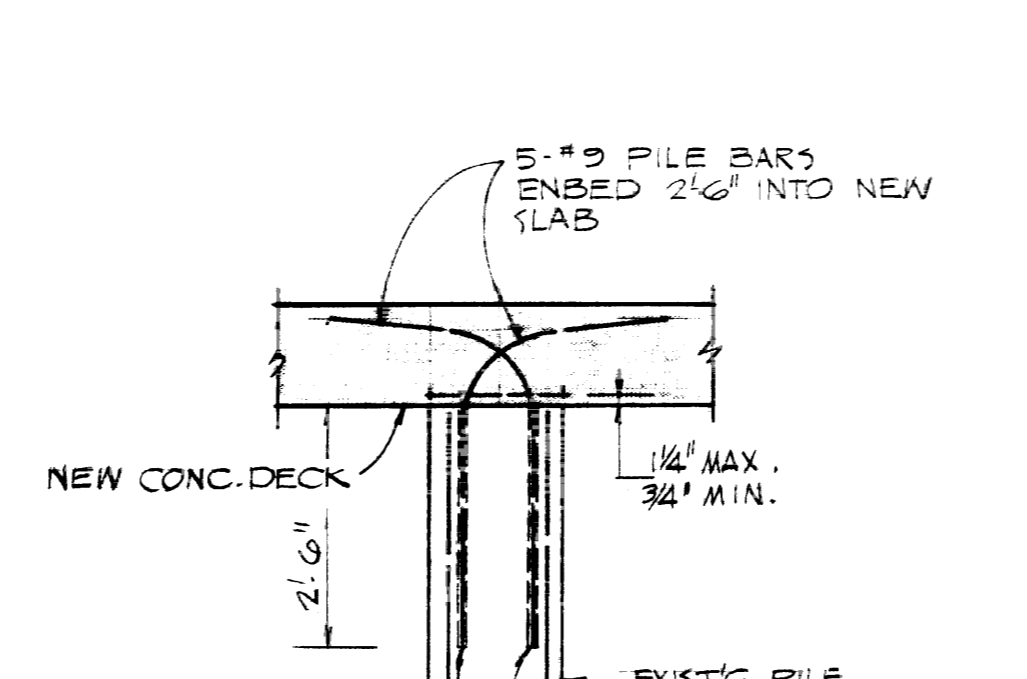
SECTION T-T
(SECTION V-V SIMILAR)
SCALE: 1/2" = 1'-0"



PLAN OF FOUNDATION BEAMS
FOR CRANE RAIL ON FILL AREA
SCALE: 1/2" = 1'-0"

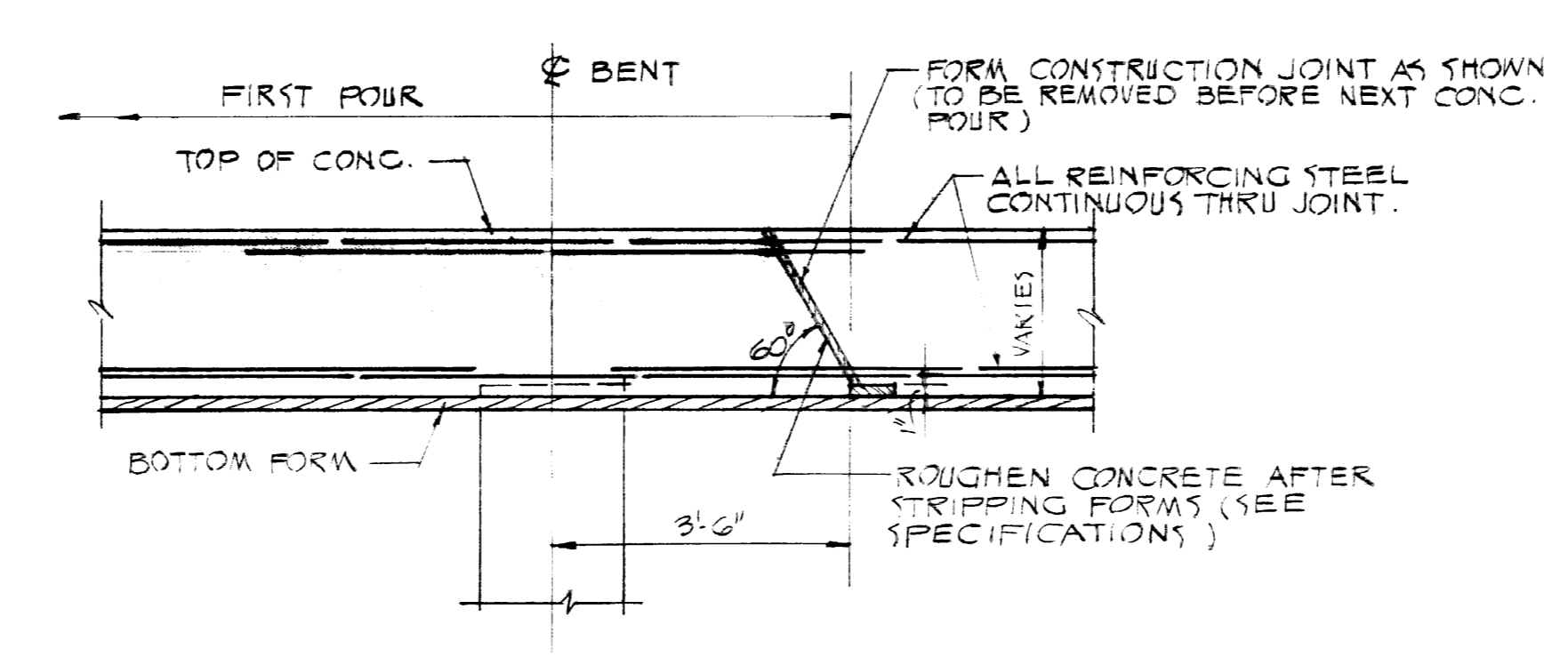


SECTION S-S
SCALE: 1/2" = 1'-0"

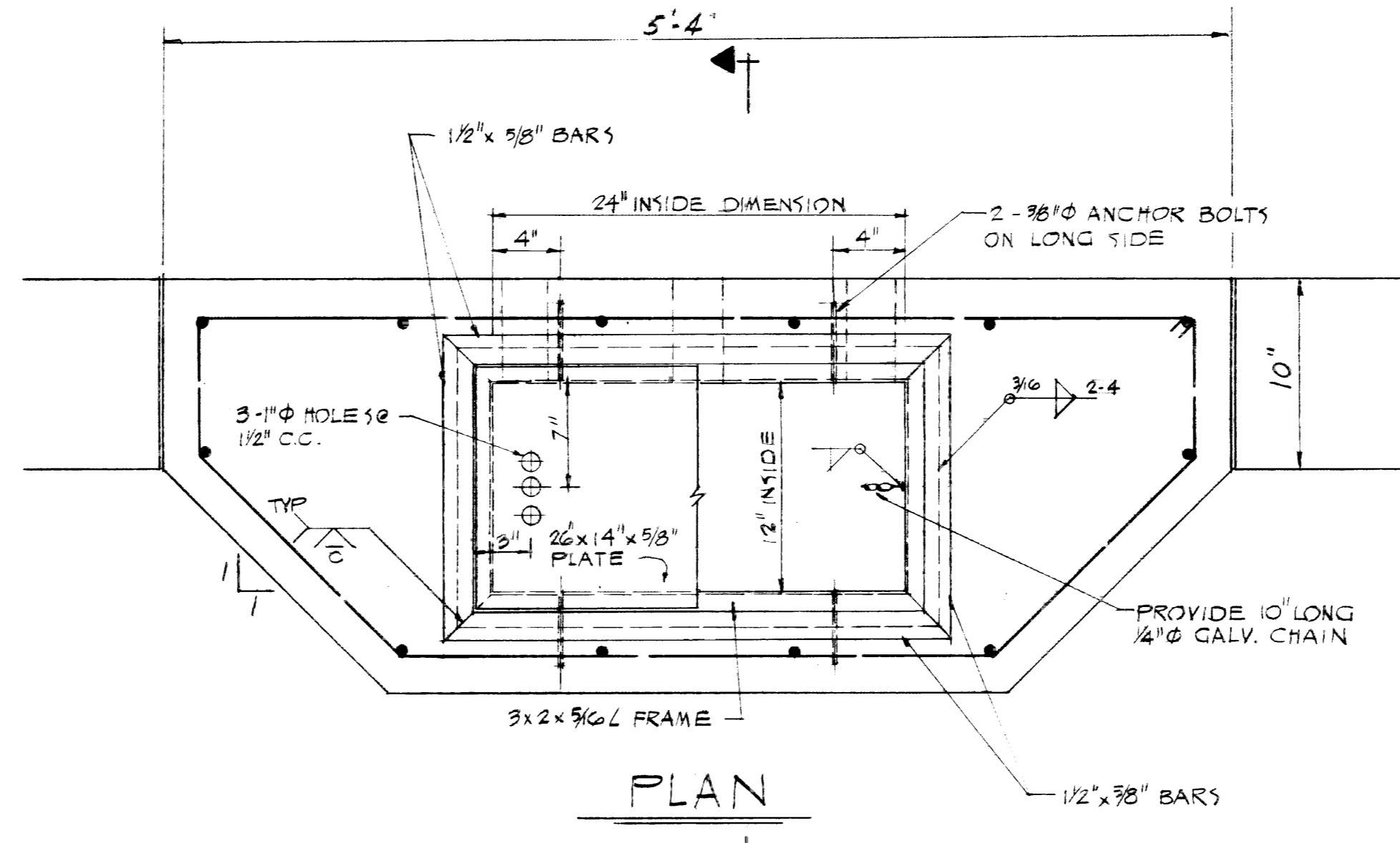


NOTE:
WHERE ORIGINAL STRANDS ARE DAMAGED BY DEMOLITION, CONTRACTOR SHALL REPLACE STRANDS WITH ONE #9 BAR FOR EVERY THREE STRANDS DAMAGED. DO NOT ADD BAR FOR TWO STRANDS DAMAGED PER PILE. ADD #9 AS SHOWN.

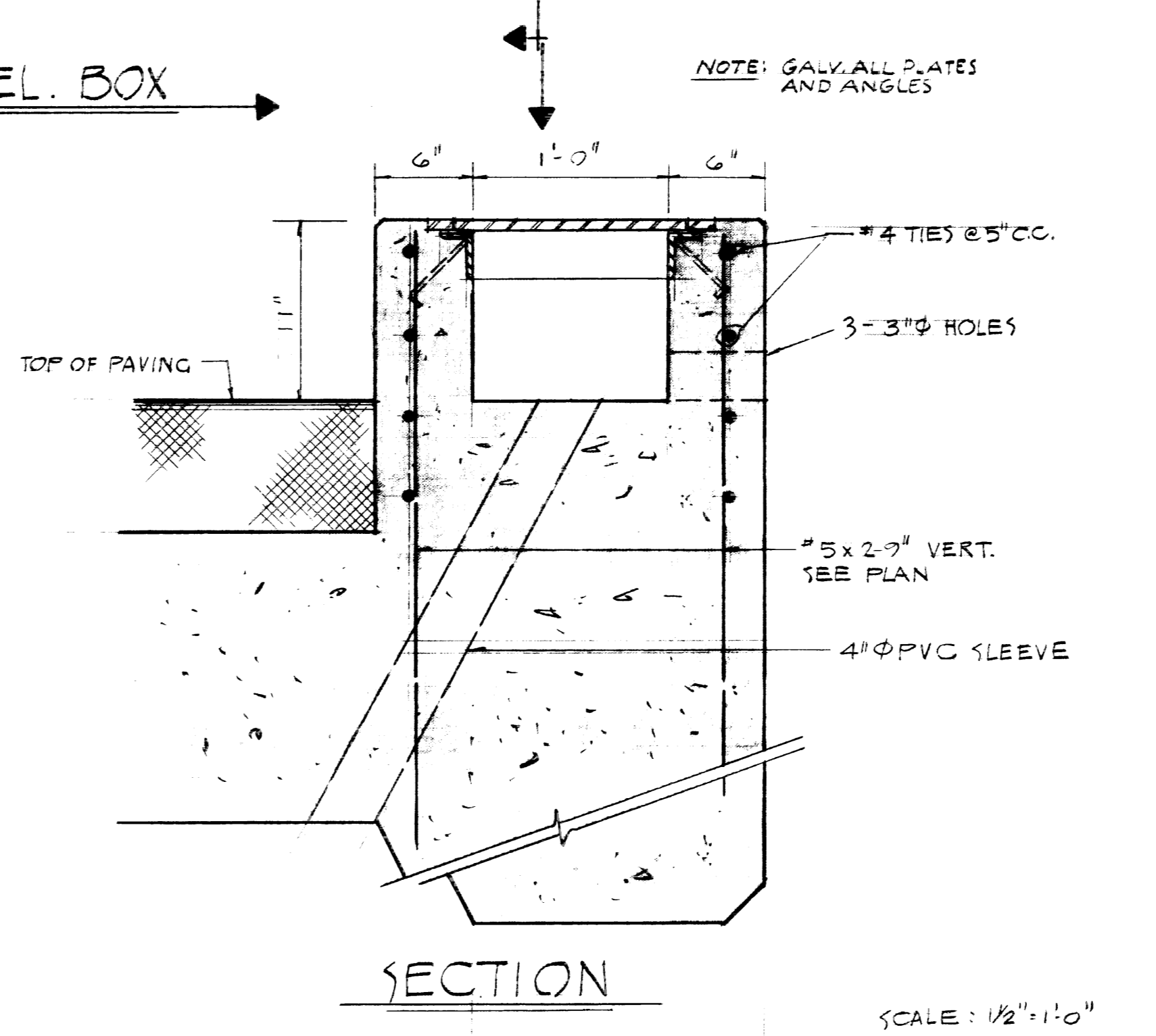
FILE HEAD DETAIL
NO SCALE



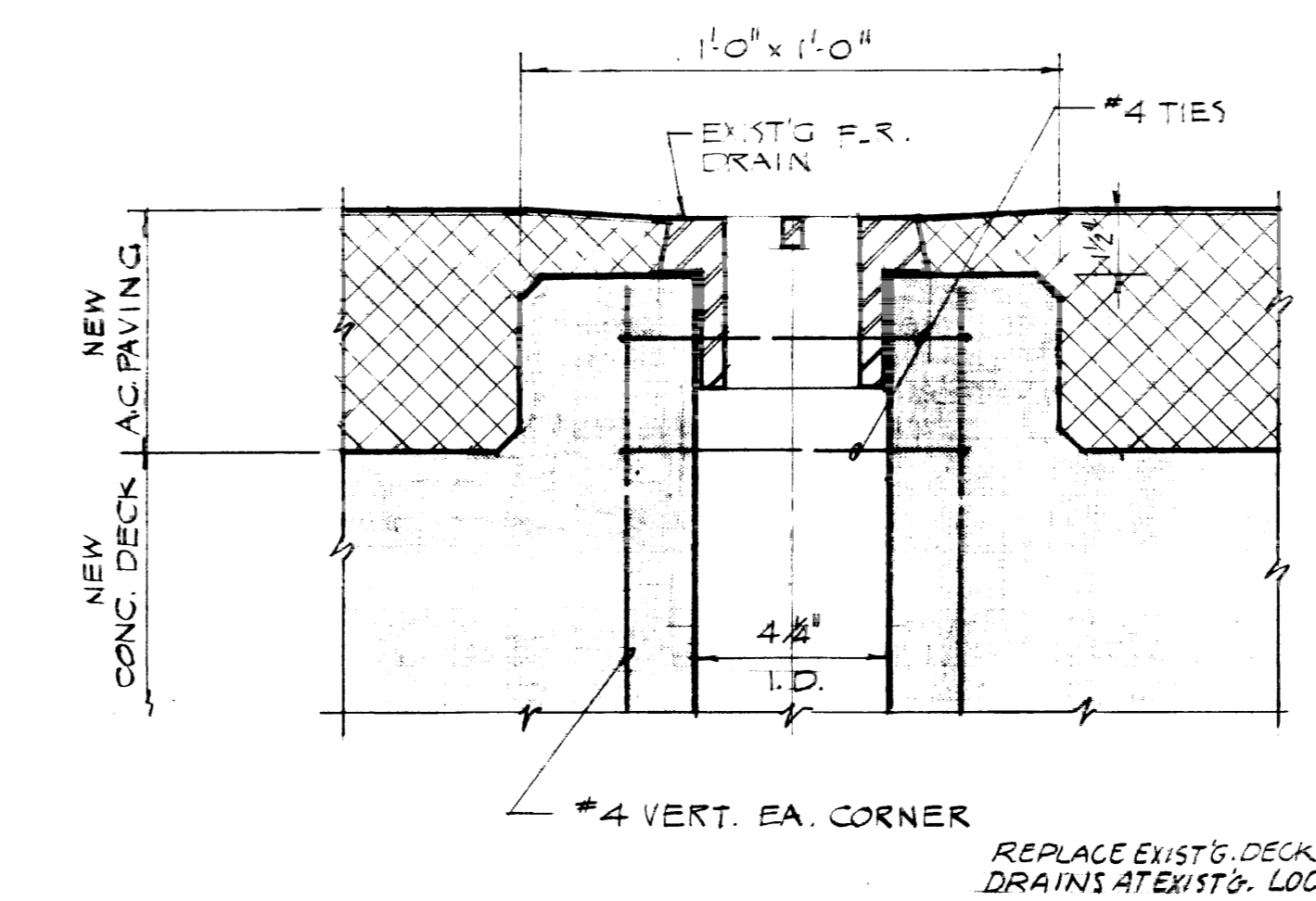
TYP. TRANSVERSE CONSTRUCTION JOINT
SCALE: 1/2" = 1'-0"



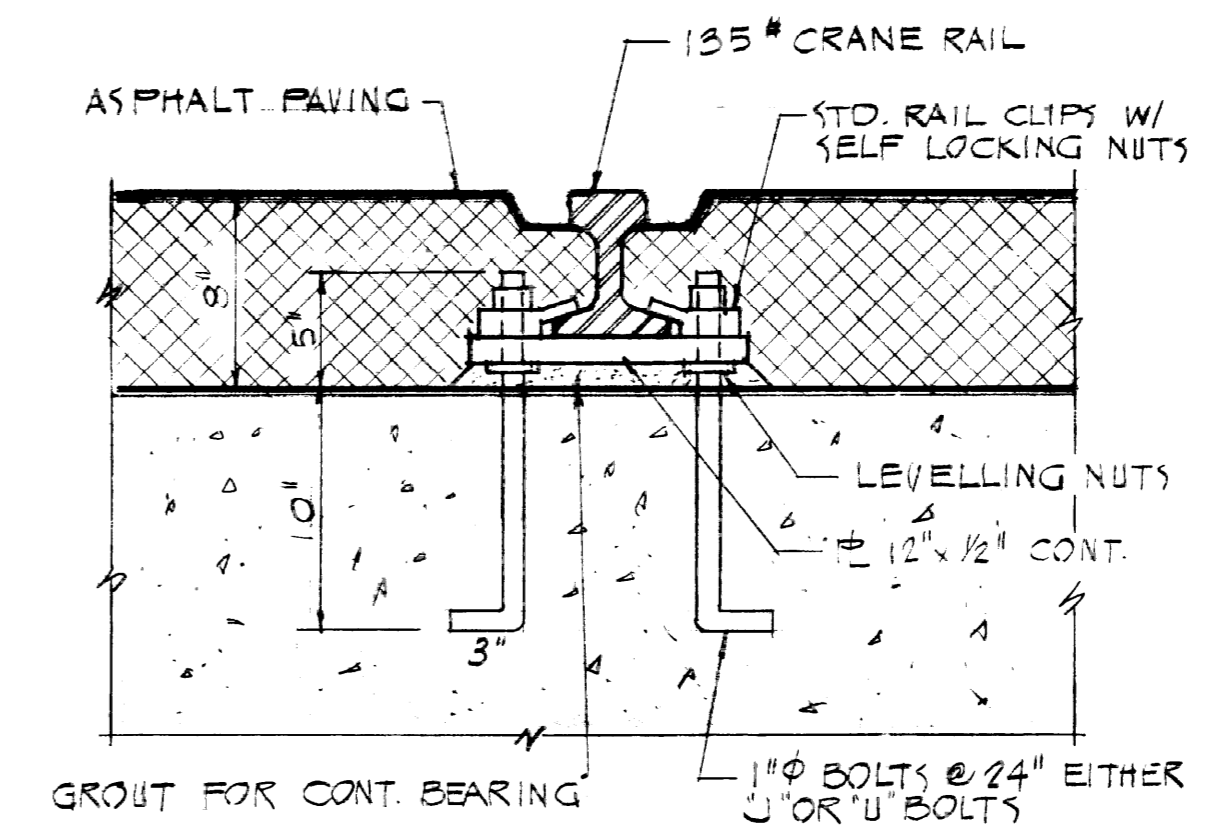
DETAIL OF TEL. BOX



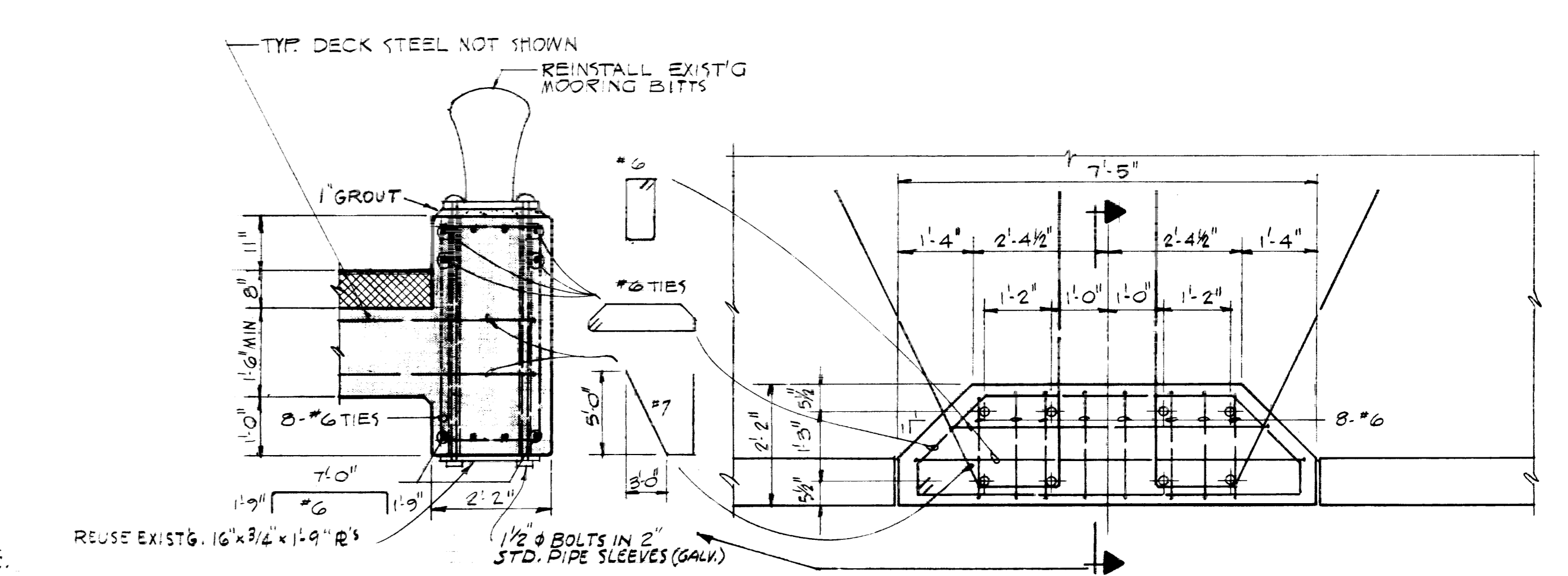
SECTION
SCALE: 1/2" = 1'-0"



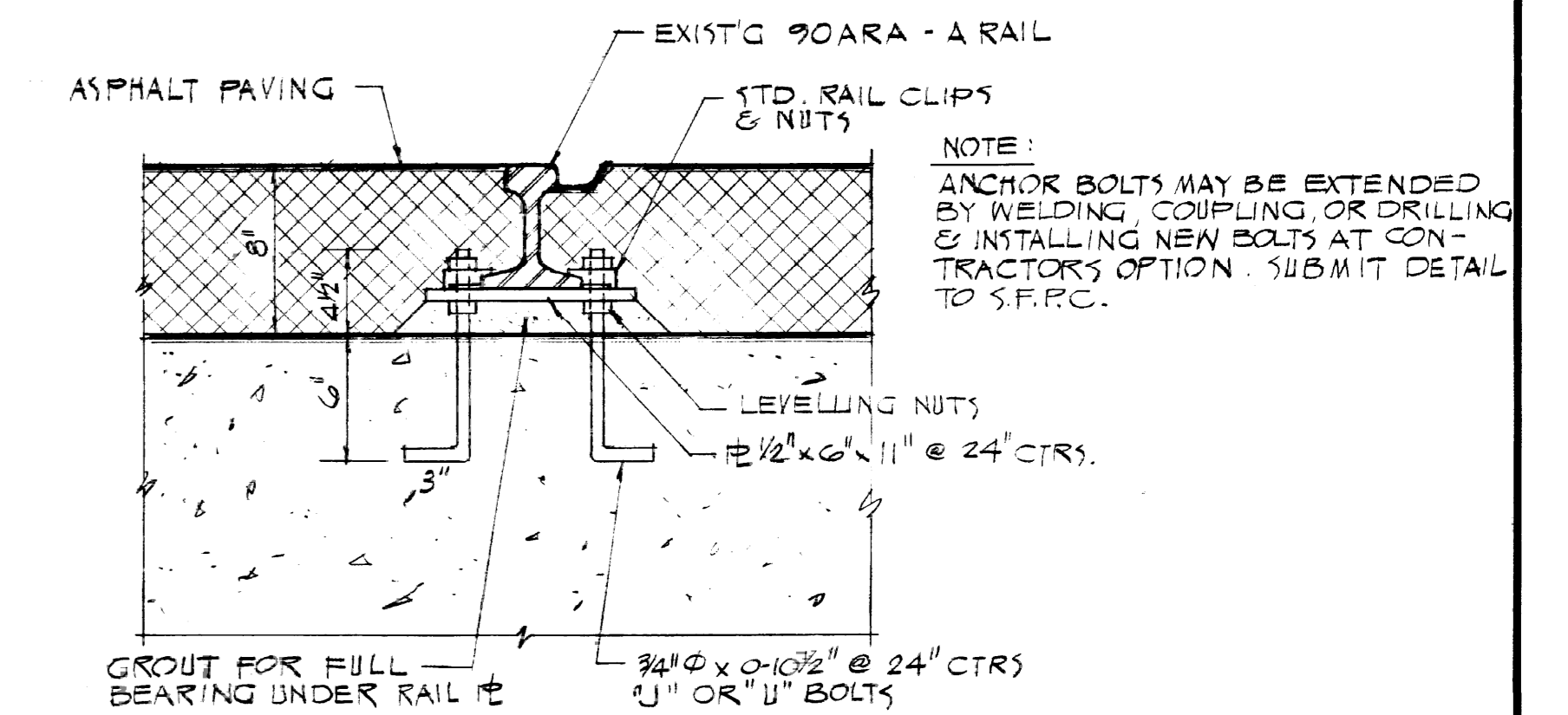
DETAIL FOR REINSTALLING EXISTING DECK DRAIN
SCALE: 8" = 1'-0"



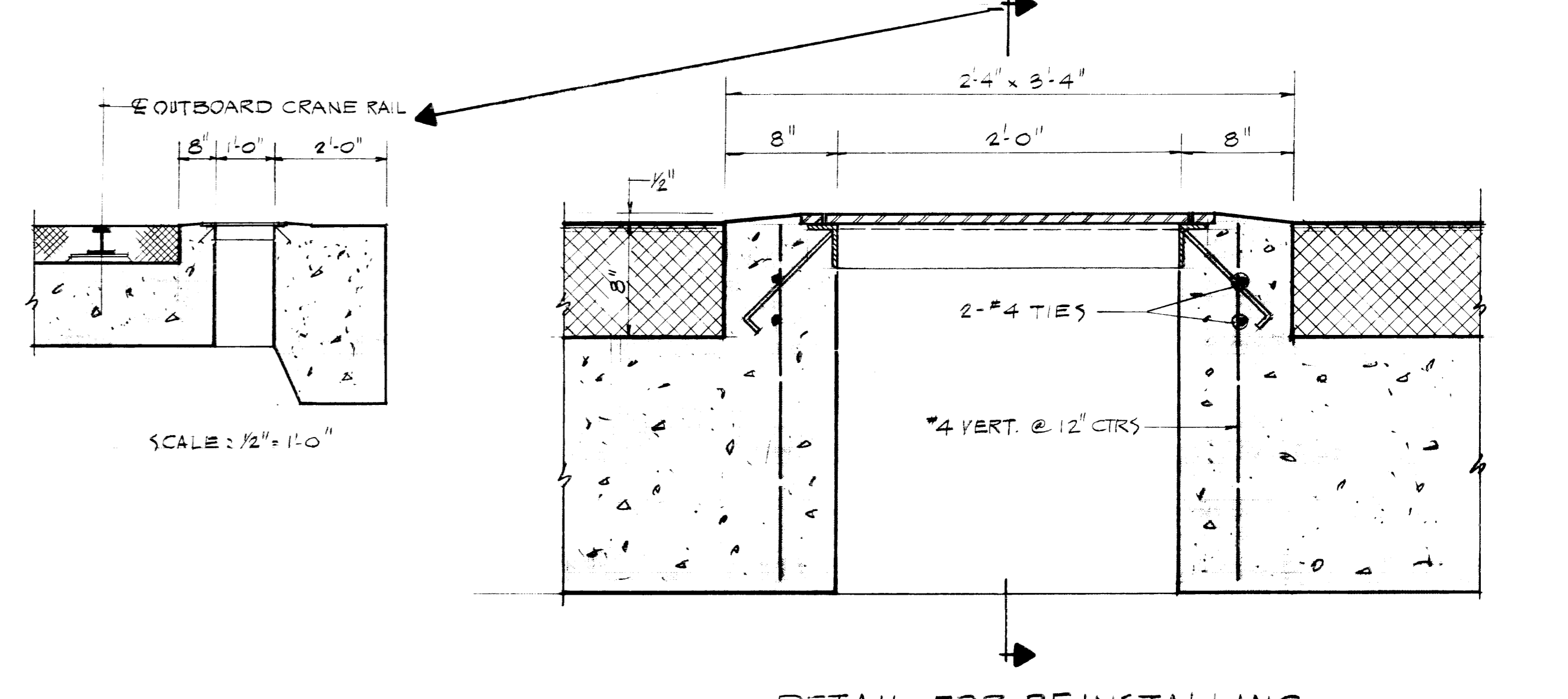
DET. OF CRANE RAIL ANCHORAGE
SCALE: 1/2" = 1'-0"



SECTION
DET. OF MOORING BOLLARD ANCHORAGE
SCALE: 1/2" = 1'-0"



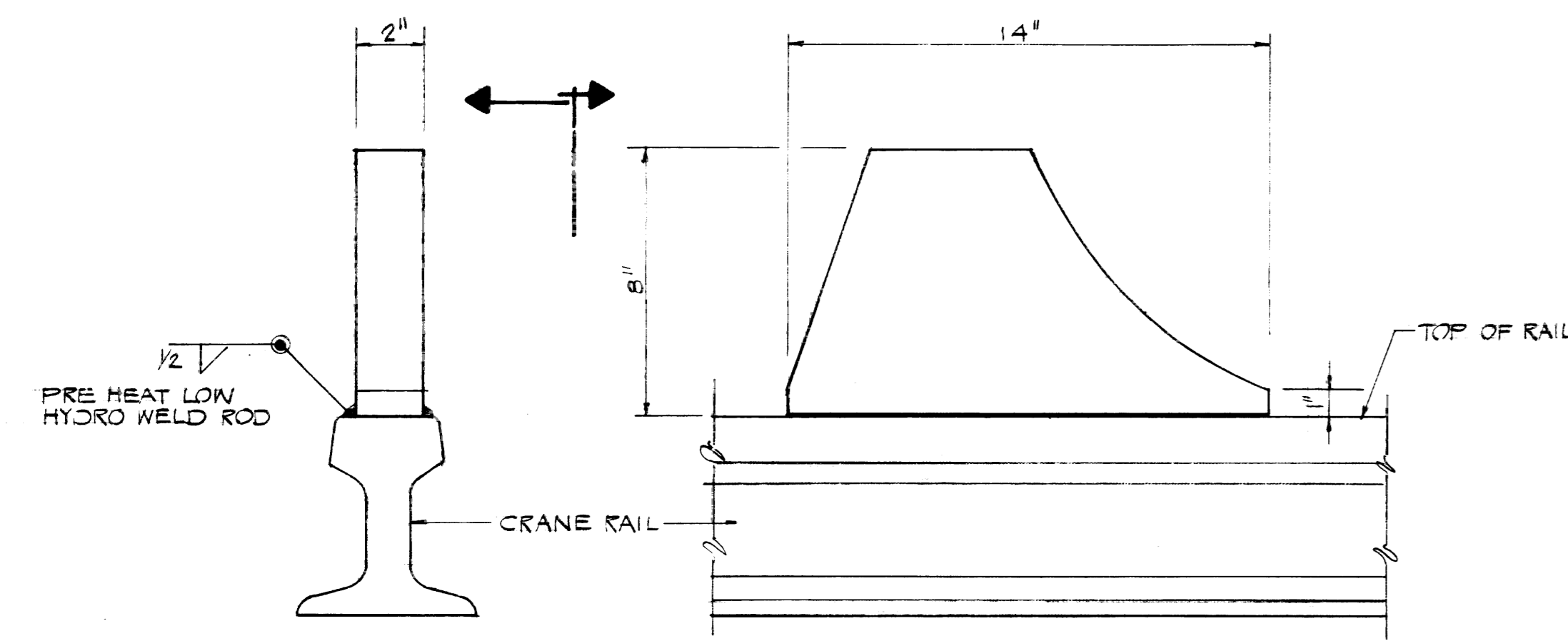
DET. OF TEE RAIL ANCHORAGE
SCALE: 1/2" = 1'-0"



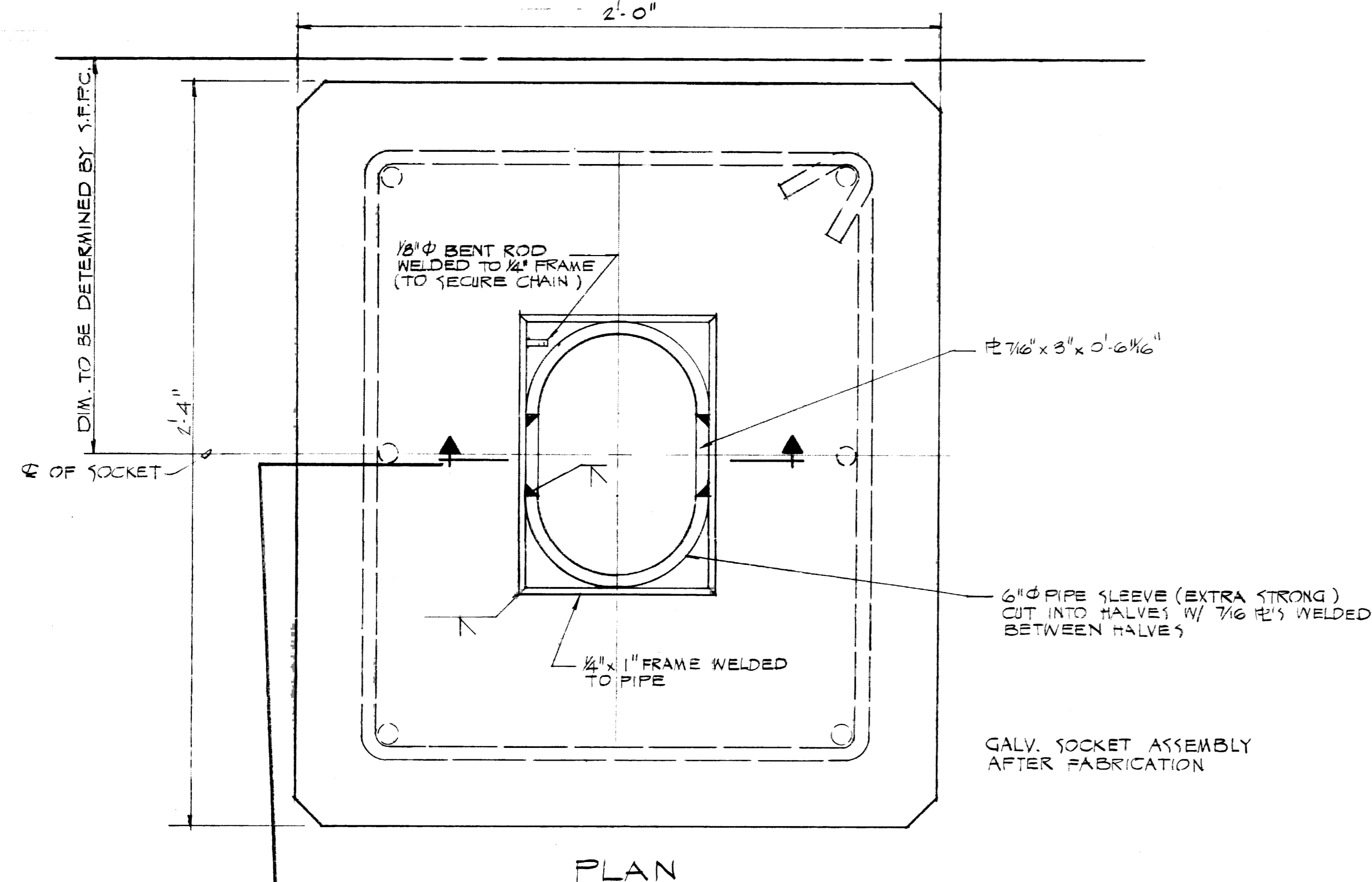
DETAIL FOR REINSTALLING EXIST'G WATER SERVICE BOX TO NEW CONG.
SCALE: 1/2" = 1'-0"

NO.	DATE	DESCRIPTION
REVISIONS		
PORT OF SAN FRANCISCO SAN FRANCISCO PORT COMMISSION DEPARTMENT OF ENGINEERING		
PIER 80 DECK MODIFICATION CURVED RAIL FOUNDATION & MISC. DETAILS		
IN CHARGE OF	W. PELBAUX	CHECKED BY
MADE BY	G. HUI	DATE
TRACED BY	G. HUI	SCALE
DRAWING NO.	8590-80-1	SHEET NO.
OF 10 SHEETS		

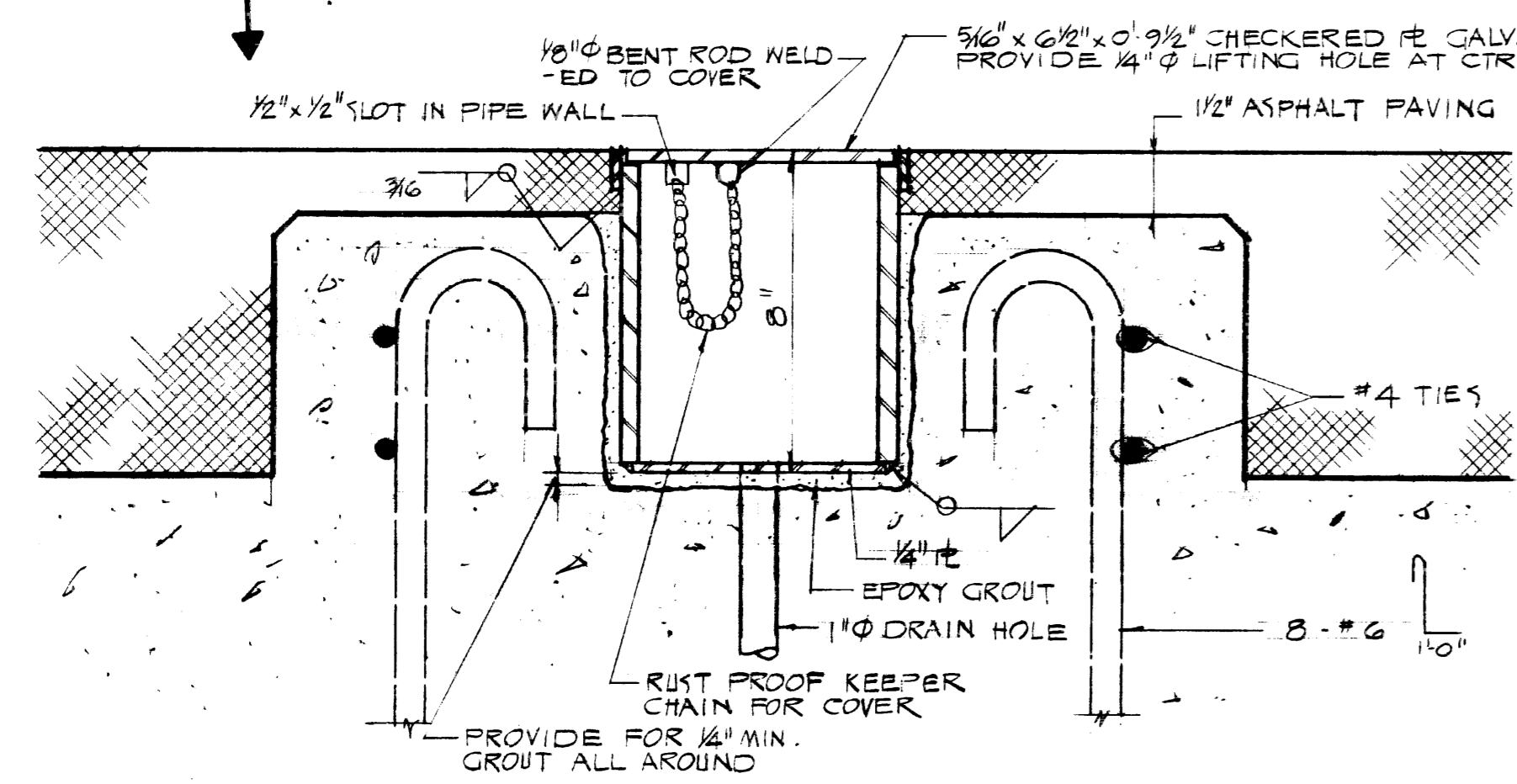
CONTRACT NO. 2422
APPROVED BY
SAN FRANCISCO PORT COMMISSION
DATE 7-24-74
C.L. Vanders
CHIEF ENGINEER



WHEEL STOP DETAIL
NO SCALE

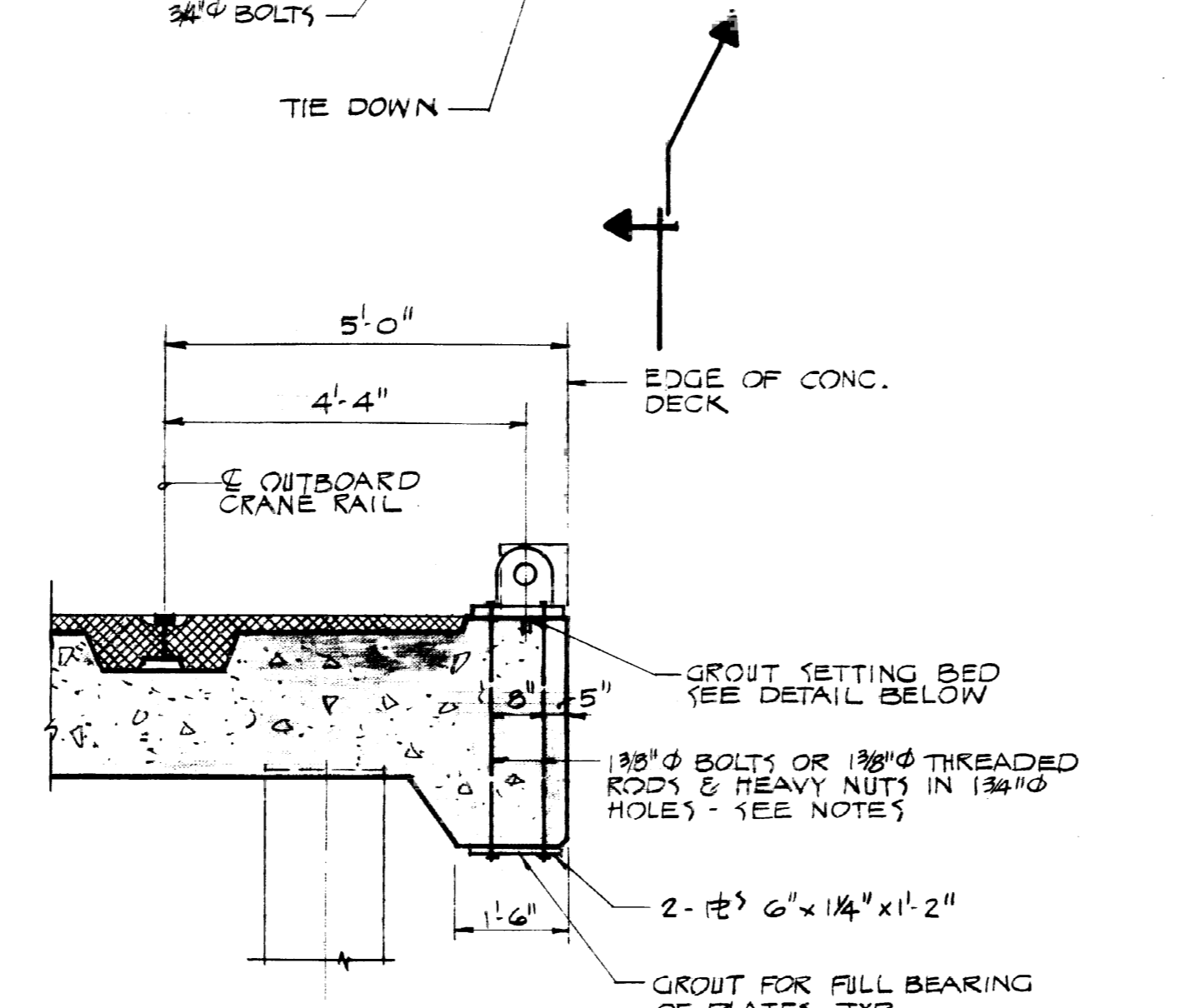
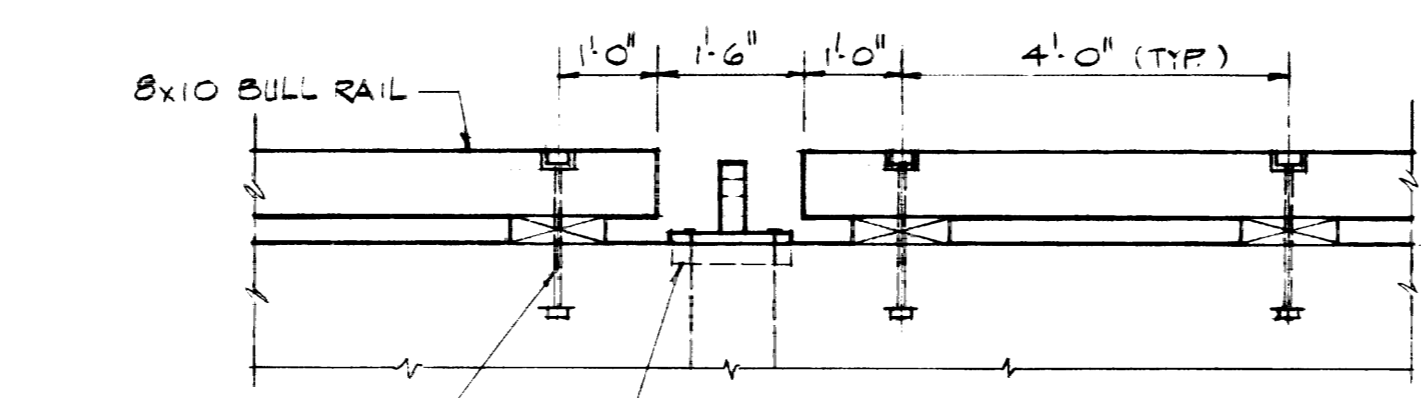


PLAN

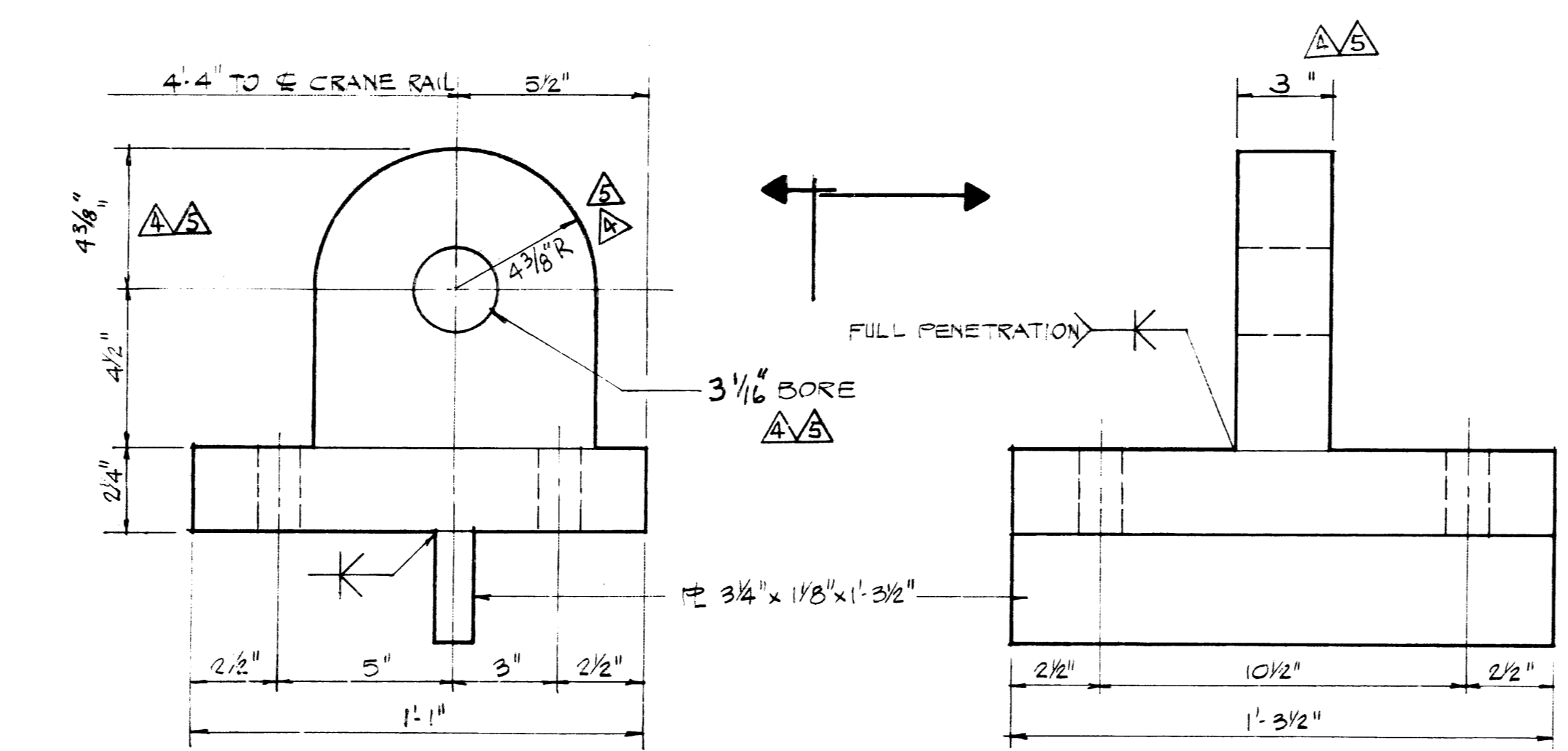


SECTION
STOWAGE PIN SOCKET DETAIL

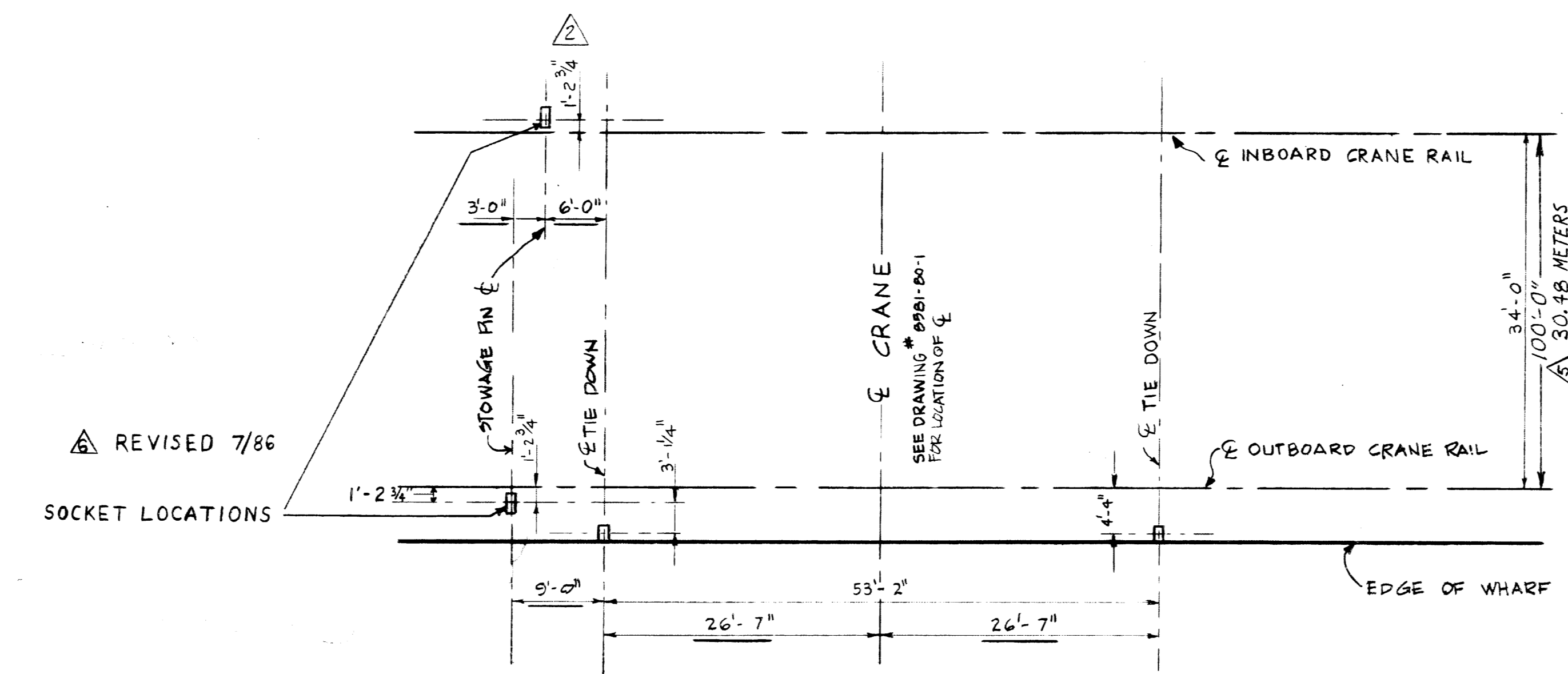
STOWAGE PIN SOCKETS AT 100' GAGE LANDSIDE RAIL WILL BE OF THIS TYPE.
SCALE: 3/4" = 1'-0"



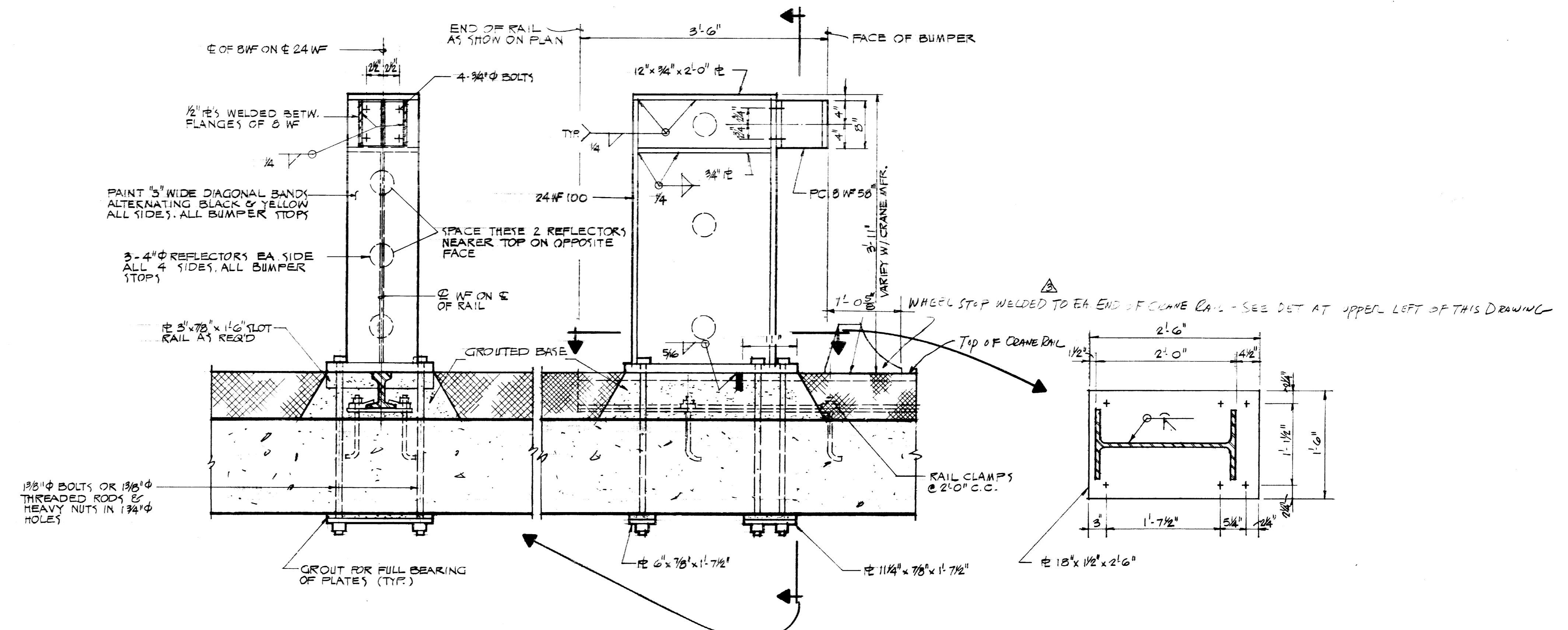
TIE DOWN INSTALLATION DETAIL
SCALE: 1/2" = 1'-0"



TIE DOWN PAD DETAIL
NOT TO SCALE



CRANE STOWAGE PIN & TIE DOWN LOCATION PLAN
SCALE: 3/32" = 1'-0"

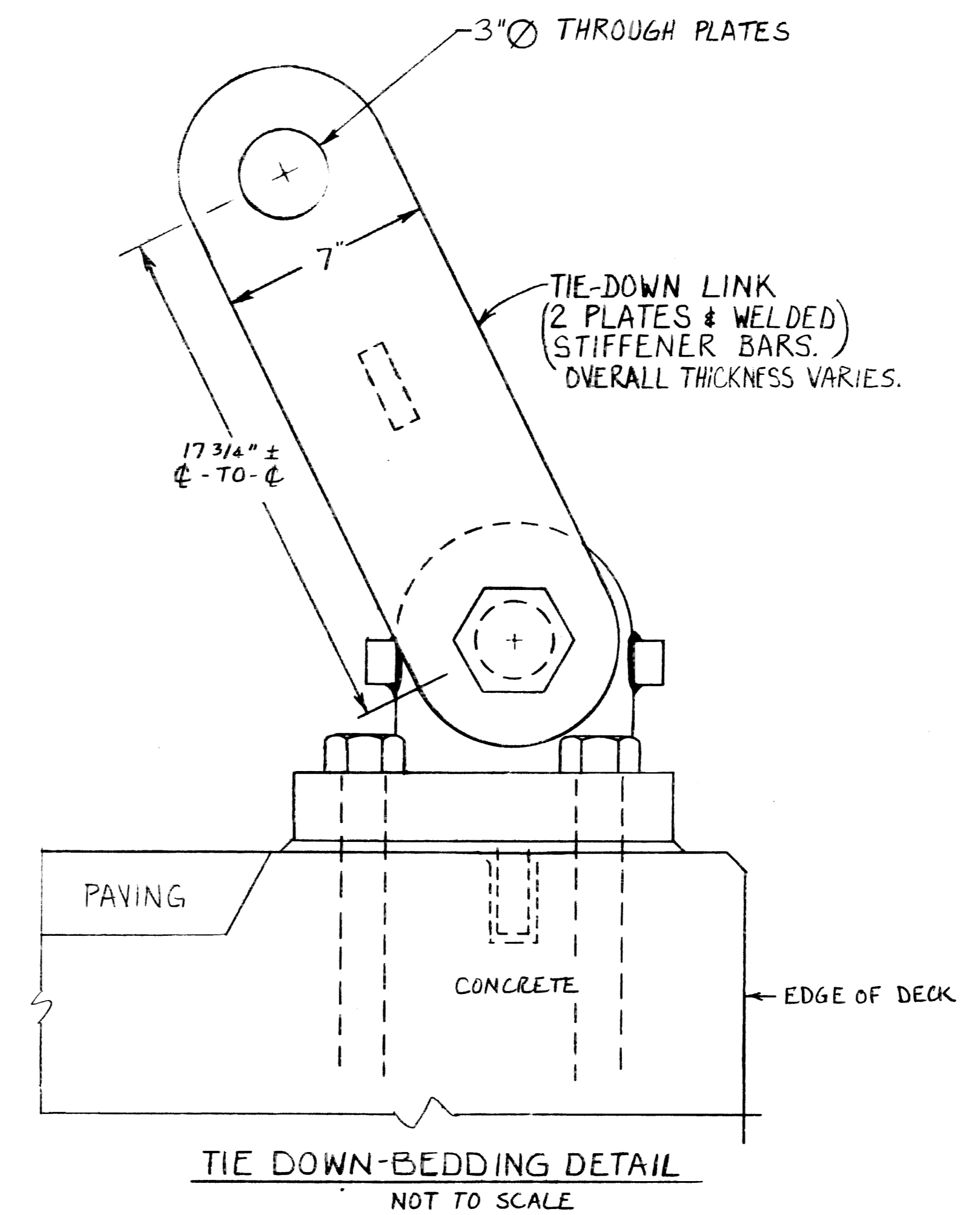


BUMPER STOP BRACKET
SCALE: 1" = 1'-0"

NEW BUMPER STOPS ON 100' GAGE LANDSIDE RAIL WILL BE OF THIS TYPE.

- NOTES
1. ALL BOLTS, RODS, & NUTS SHALL CONFORM TO A.S.T.M. A325 SPEC.
 2. ALL STRUCTURAL STEEL SHALL CONFORM TO A.S.T.M. A588 SPEC.
 3. ALL WELDING SHALL BE IN CONFORMANCE WITH THE CURRENT A.W.S. STD. SPEC. FOR WELDED HIGHWAY AND RAILROAD BRIDGES.
 4. ALL BOLTS THRU DECK, IF AT UNDER SIDE OF DECK AND OTHER METAL AS NOTED SHALL BE GALVANIZED.
 5. ALL METAL NOT TO BE GALVANIZED SHALL BE PAINTED 2 COATS SLOW DRYING RED LEAD IN ACCORDANCE WITH FED. SPEC. TT-P-86 TYPE 1 AND 2 COATS ALKYD PAINT IN ACCORDANCE WITH SSPC PAINT 104 PAINT BY I.F.P.C.
 6. GROUT SHALL BE A NON SHRINK GROUT SUCH AS EMBECO PREMIXED GROUT OR FOR-KOK, HALLEWITE CO. OR APPROVED EQUAL.
 7. TIE DOWN PADS, STOWAGE PIN SOCKETS, AND BUMPER STOP BRACKET LOCATIONS, TO BE DETERMINED BY I.F.P.C. AND VERIFIED IN FIELD FROM ACTUAL MEASUREMENTS OF CONTAINER CRANE WHICH IS BEING CONSTRUCTED AT SITE.

NOT IN CONTRACT 2532.

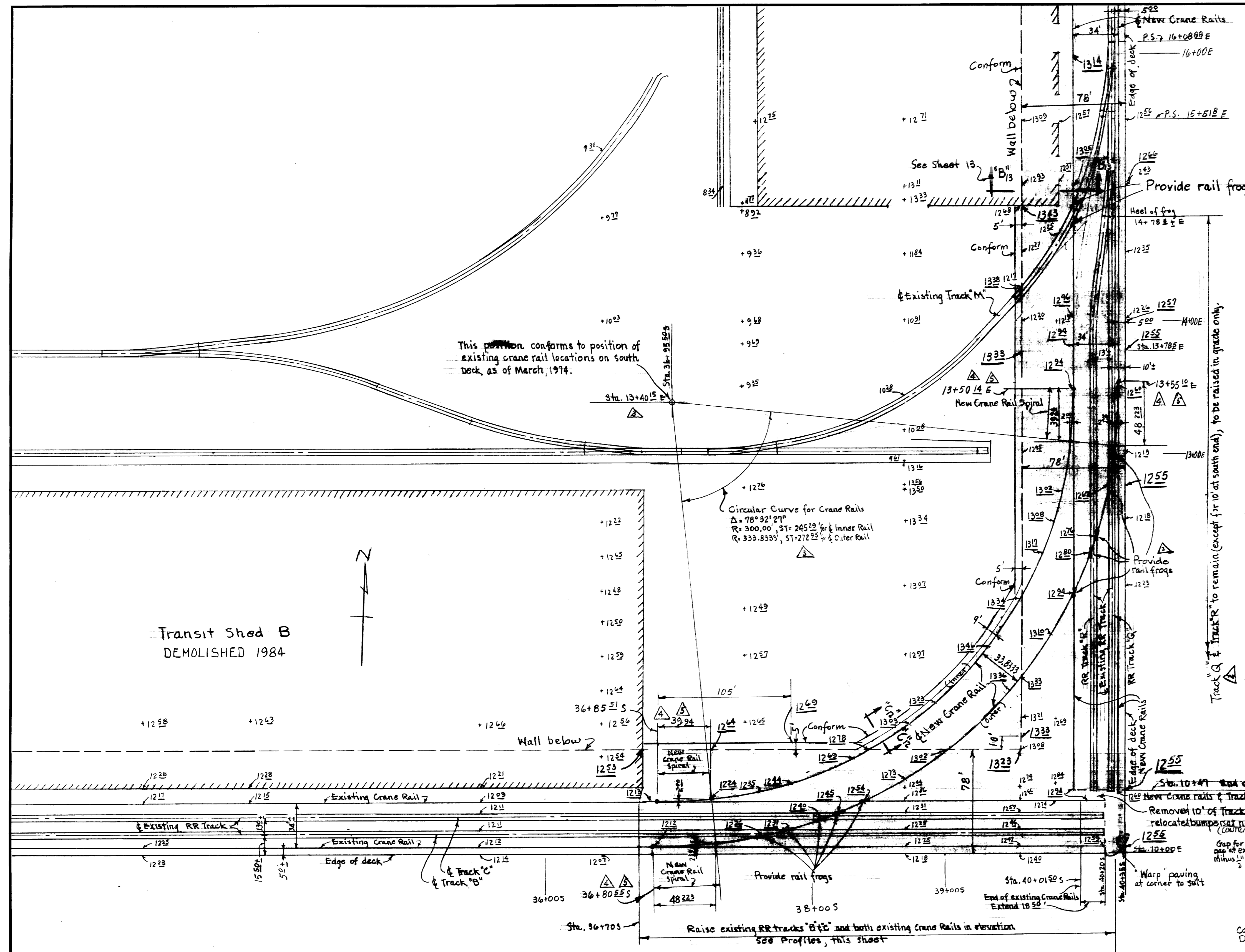


TIE DOWN-BEDDING DETAIL
NOT TO SCALE

NO.	DATE	DESCRIPTION
7/86		REVISED WS SOCKET
1/85		PREPARED DWG. FOR #2532
6/83		REVISED PADEYE
9/23/72		LOCATION OF WHEEL STOP ADDED
8/28/70		ADDED DIMENSION TO STOWAGE PIN & TIE DOWN LOCATION PLAN.
		TIE DOWN PAD & STOWAGE PIN LOCATED FROM PLANNING OF THE DECK.
REVISIONS		
PORT OF SAN FRANCISCO SAN FRANCISCO PORT COMMISSION DEPARTMENT OF ENGINEERING		
PIER 80 DECK MODIFICATION STOWAGE PIN & BUMPER STOP DETAILS		

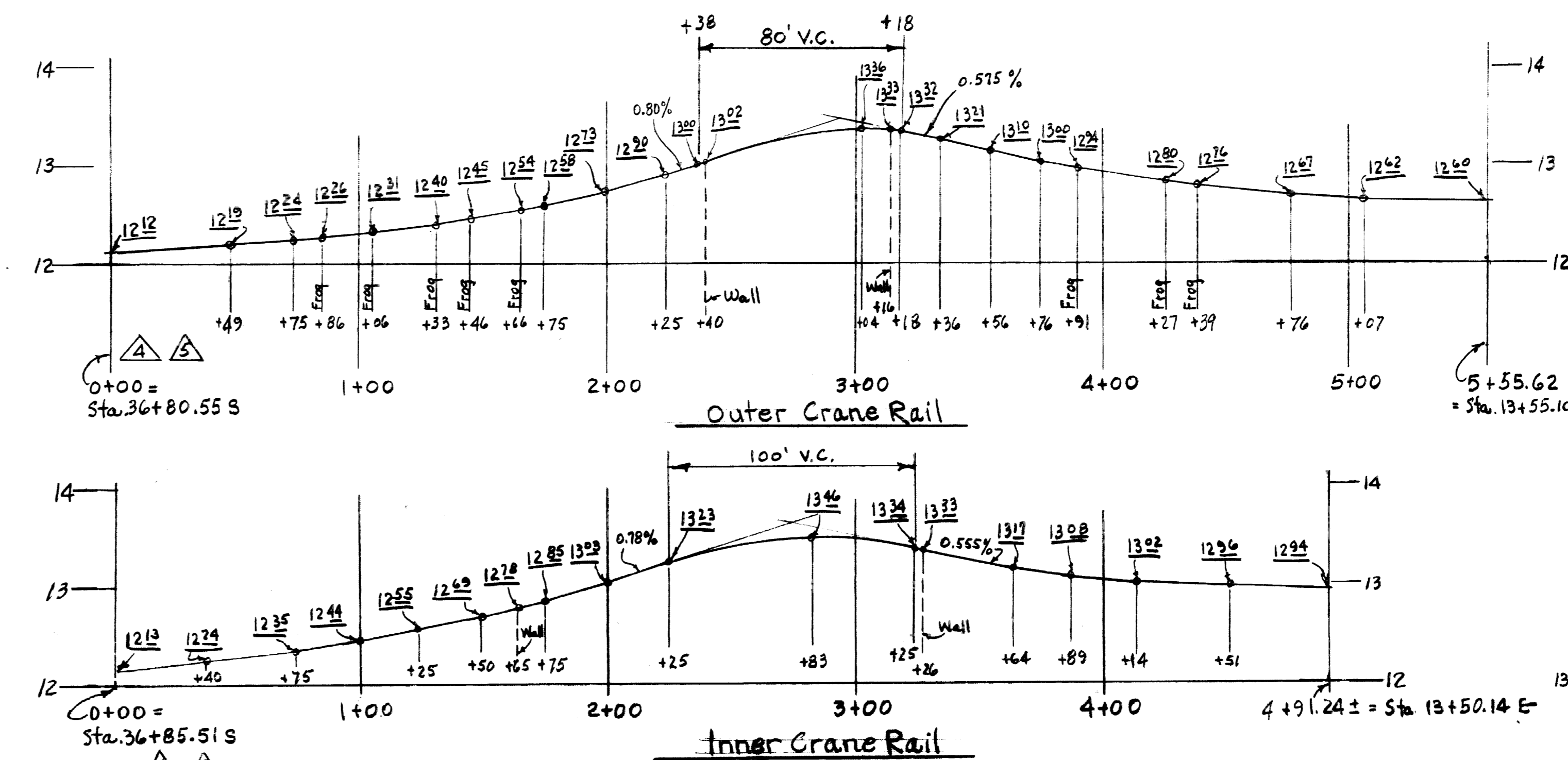
CONTRACT 2532 & CONTRACT NO. 24 22

APPROVED BY SAN FRANCISCO PORT COMMISSION DATE: 7-24-74	IN CHARGE OF W. F. LEALUX MADE BY G. HUI DATE: 7-24-74	CHECKED BY T. WOODRUFF DATE: 7-24-74	SCALE: AS NOTED
DRAWING NO. 8591-80-1		SHEET NO. 11 OF 14 SHEETS	

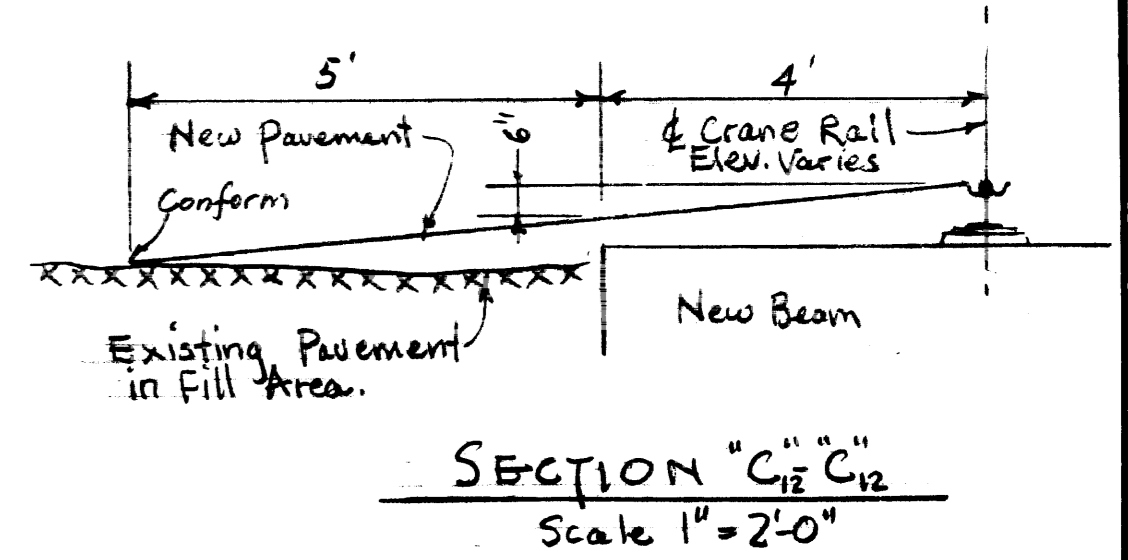


Transit Shed B
DEMOLISHED 1984

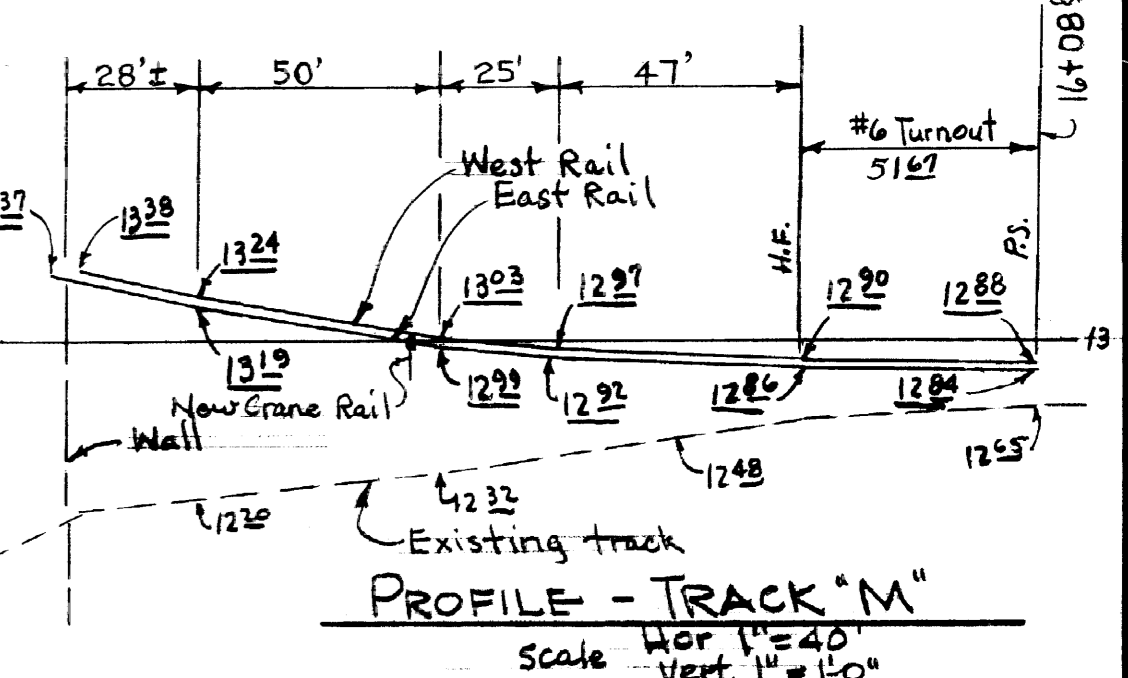
DETAIL PLAN (SOUTH HALF)
Scale 1" = 40'



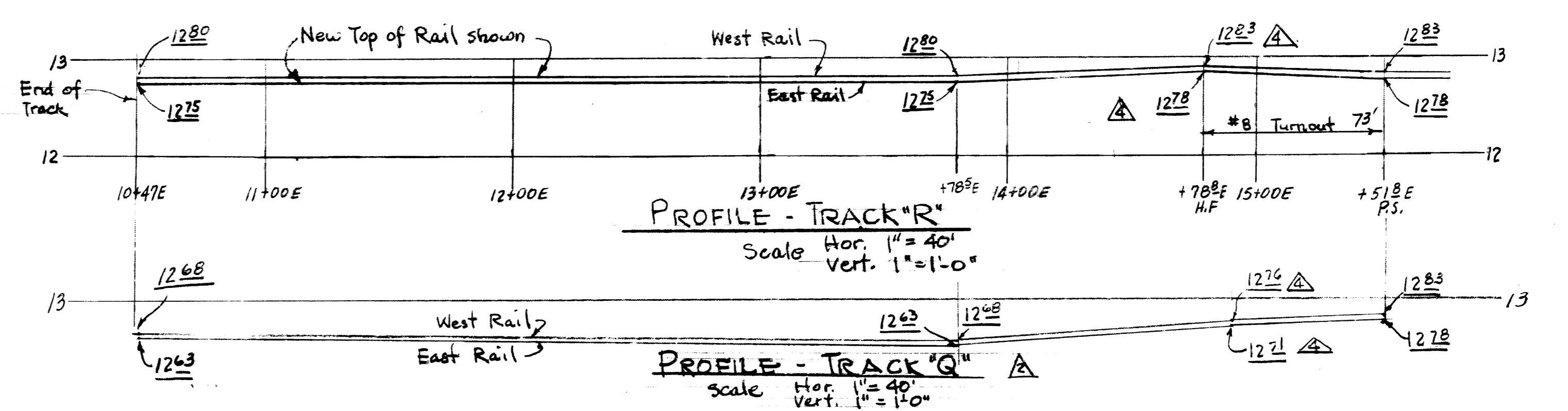
NEW CURVED CRANE RAIL PROFILES
Scale: Hor. 1" = 40', Vert. 1" = 1'-0"
Note - For profiles of New Crane Rails on East & North Decks: See Dwg. No. 8581-80-1



SECTION "C-C"
Scale 1" = 2'-0"

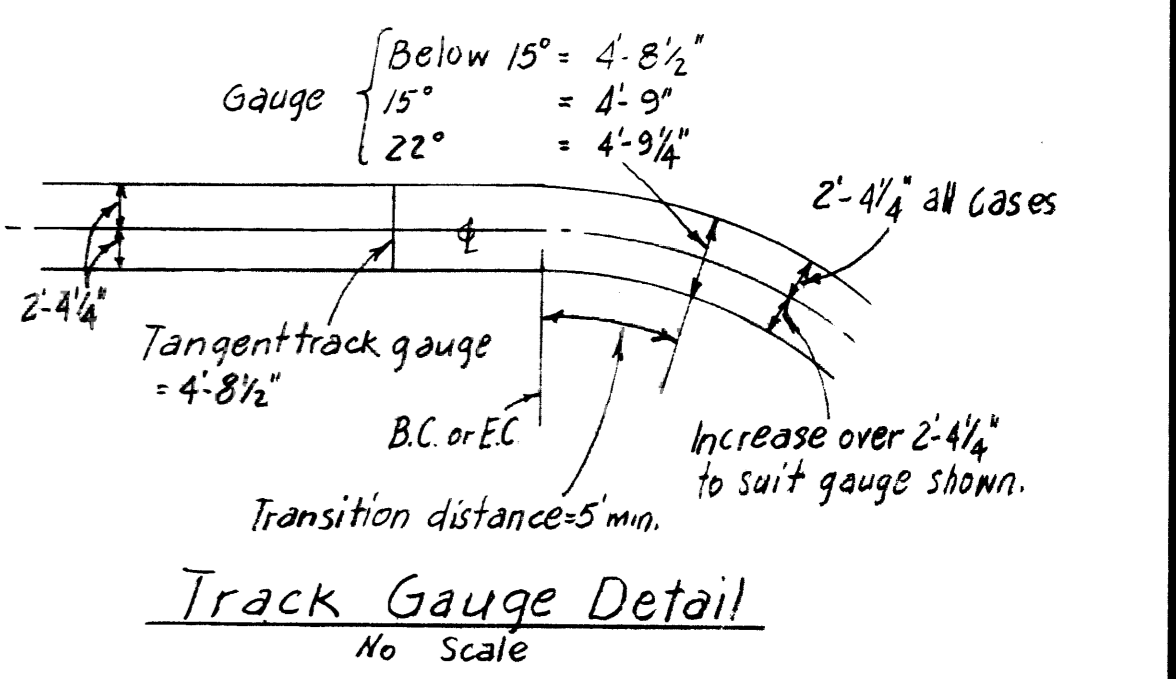


PROFILE - TRACK "M"
Scale: Hor. 1" = 10', Vert. 1" = 1'-0"

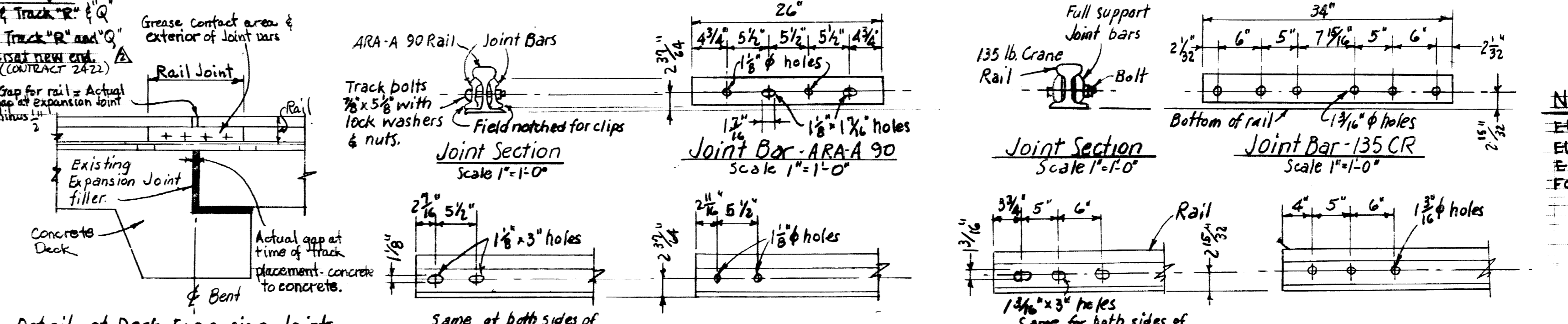


PROFILE - TRACK "R"
Scale: Hor. 1" = 40', Vert. 1" = 1'-0"

PROFILE - TRACK "Q"
Scale: Hor. 1" = 40', Vert. 1" = 1'-0"



Track Gauge Detail
No Scale



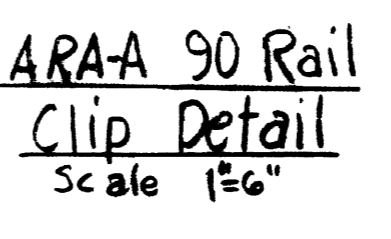
Detail at Deck Expansion Joints
Scale 1" = 1'-0" (Crane Rail Similar)

Rail Drilling - ARA 90
Scale 1" = 1'-0" (Expansion Joints Only)

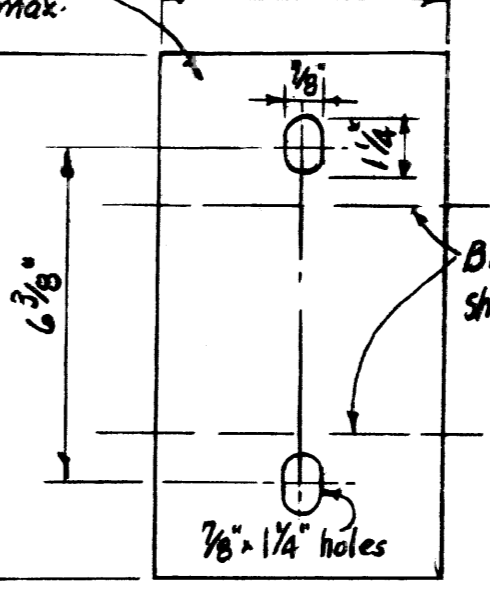
Rail Drilling - ARA 90
Scale 1" = 1'-0" (Except at Expansion Joints)

Rail Drilling 135 CR
Scale 1" = 1'-0" (Expansion Joints Only)

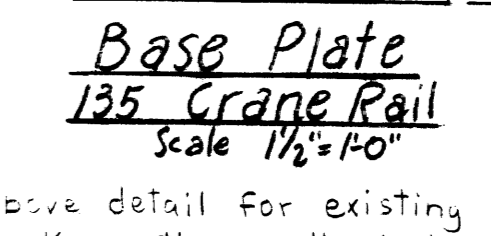
Rail Drilling 135 CR
Scale 1" = 1'-0" (Except at Expansion Joints)



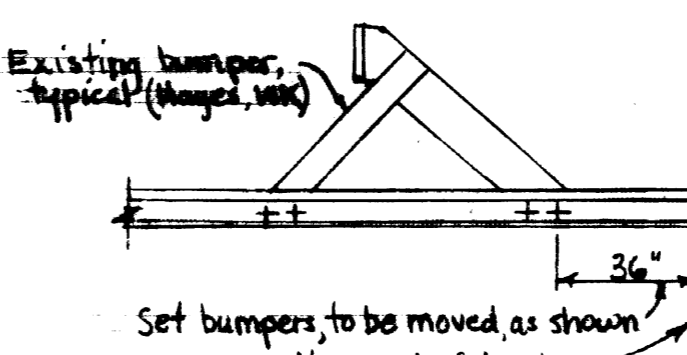
ARA 90 RAIL CLIP DETAIL
Scale 1" = 1'-0"



Base Plate ARA 90 RAIL
Scale: 3" = 1'-0"

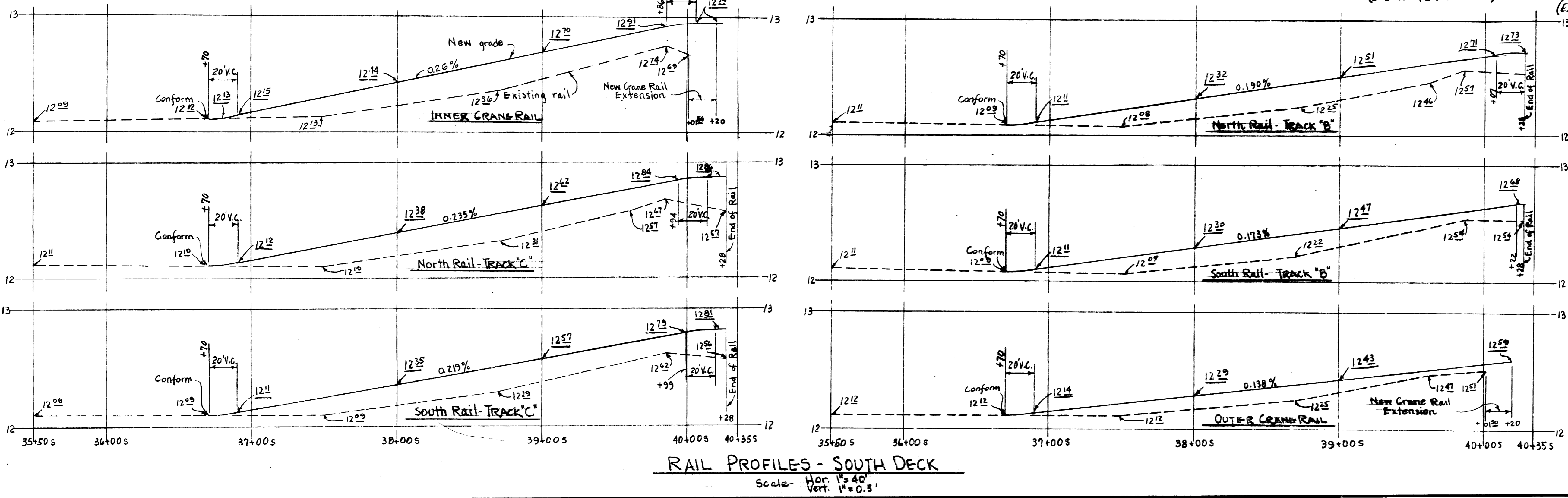


Base Plate 135 CRANE RAIL
Scale 1 1/2" = 1'-0"



Bumper Detail
Scale 1/4" = 1'-0"

Note - Rail Details above show typical existing details.



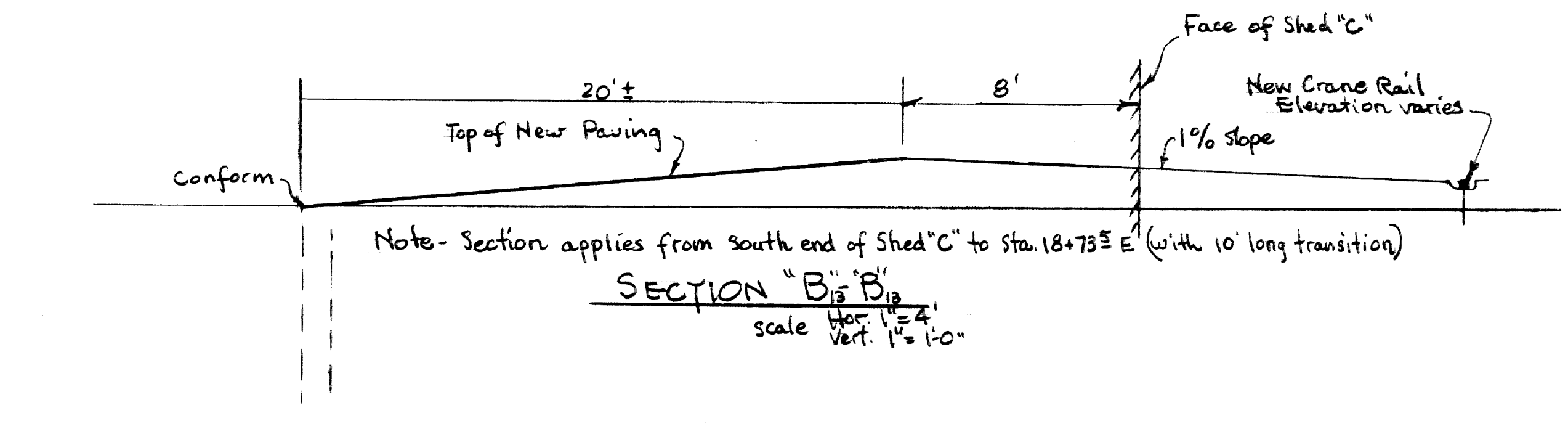
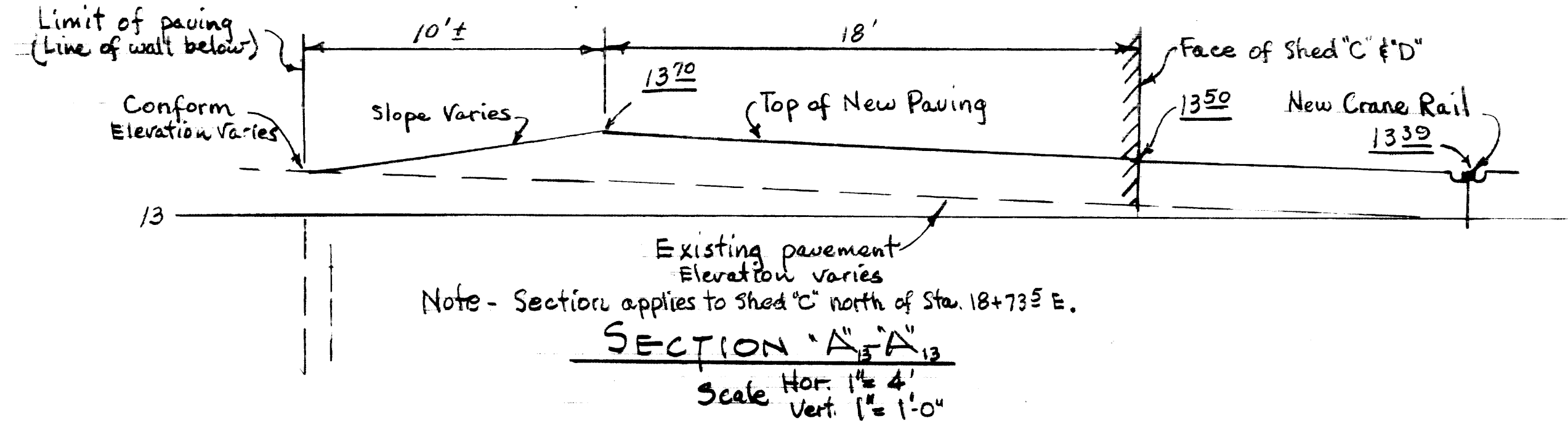
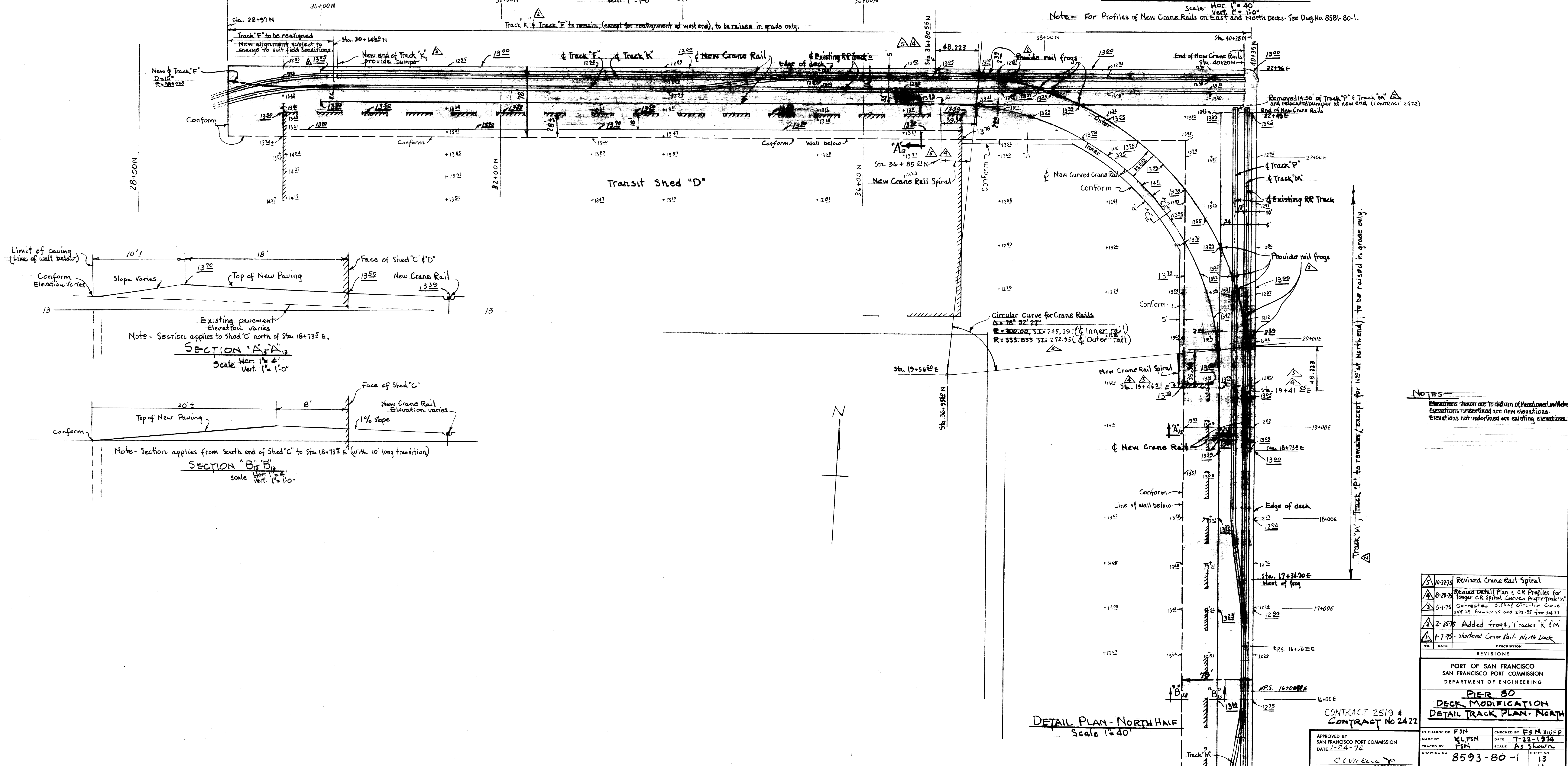
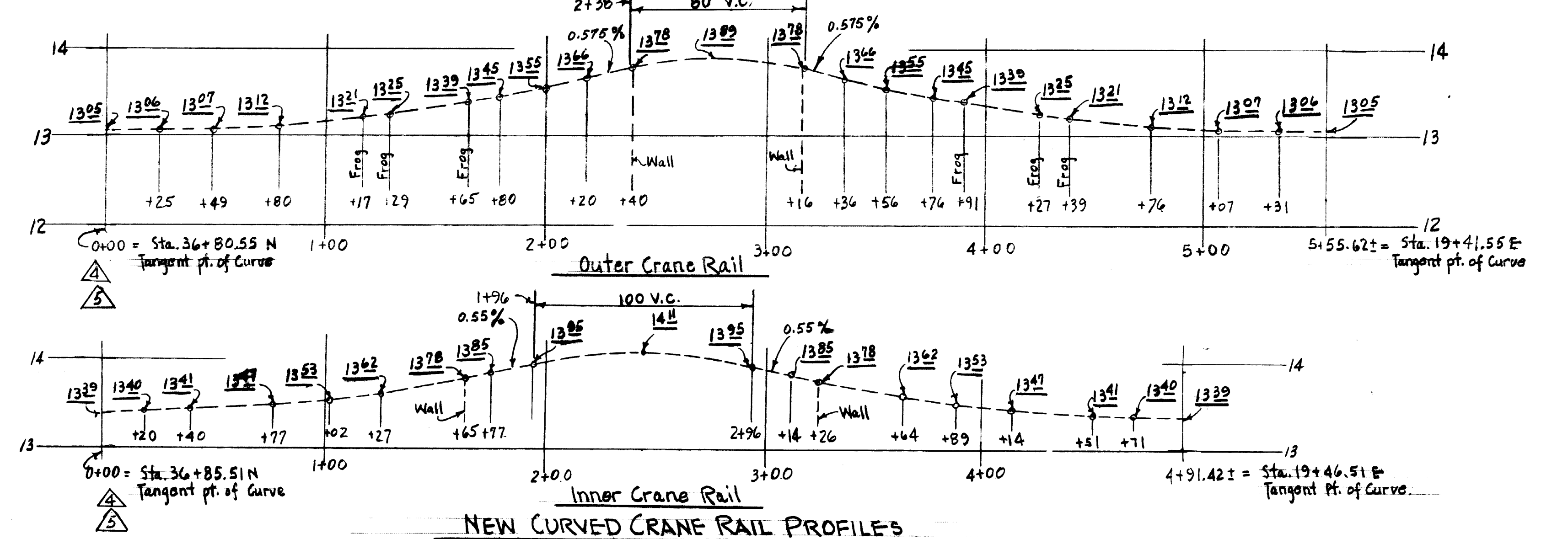
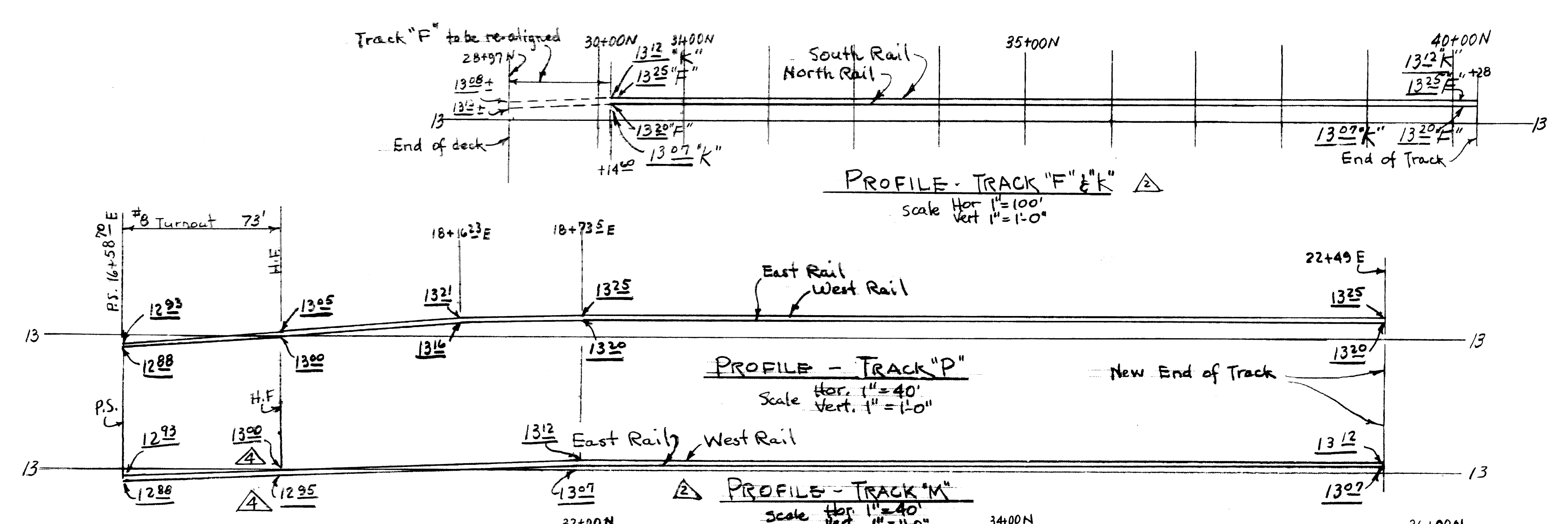
RAIL PROFILES - SOUTH DECK
Scale: Hor. 1" = 40', Vert. 1" = 0'-5"

NOTES
Elevations shown are to datum of Mean Lower Low Water (MLLW) below City Datum. Elevations underlined are new elevations. Elevations not underlined are existing elevations. For General Plan, See Dwg. No. 8581-80-1.

NO.	DATE	DESCRIPTION
1	7-22-74	Revised Crane Rail Spiral
2	8-20-75	Revised Detail Plan CR Profiles for Longer CR Spirals, Profile Track C & R.
3	6-11-76	Corrected S.T. and center coordinate of circular curve.
4	2-28-78	Added Track 'Q' changed base plate for crane rail.

REVISIONS	
NO.	DESCRIPTION
1	CONTRACT 2430 & 2434

APPROVED BY SAN FRANCISCO PORT COMMISSION DATE 7-28-74		CHECKED BY ESN WLF DATE 7-22-1974	
DRAWING NO. 8592-80-1		SCALE As shown	
SHEET NO. 12		OF 12 SHEETS	



NOTES
 Elevations shown are to datum of Mean Lower Low Water
 Elevations underlined are new elevations.
 Elevations not underlined are existing elevations.

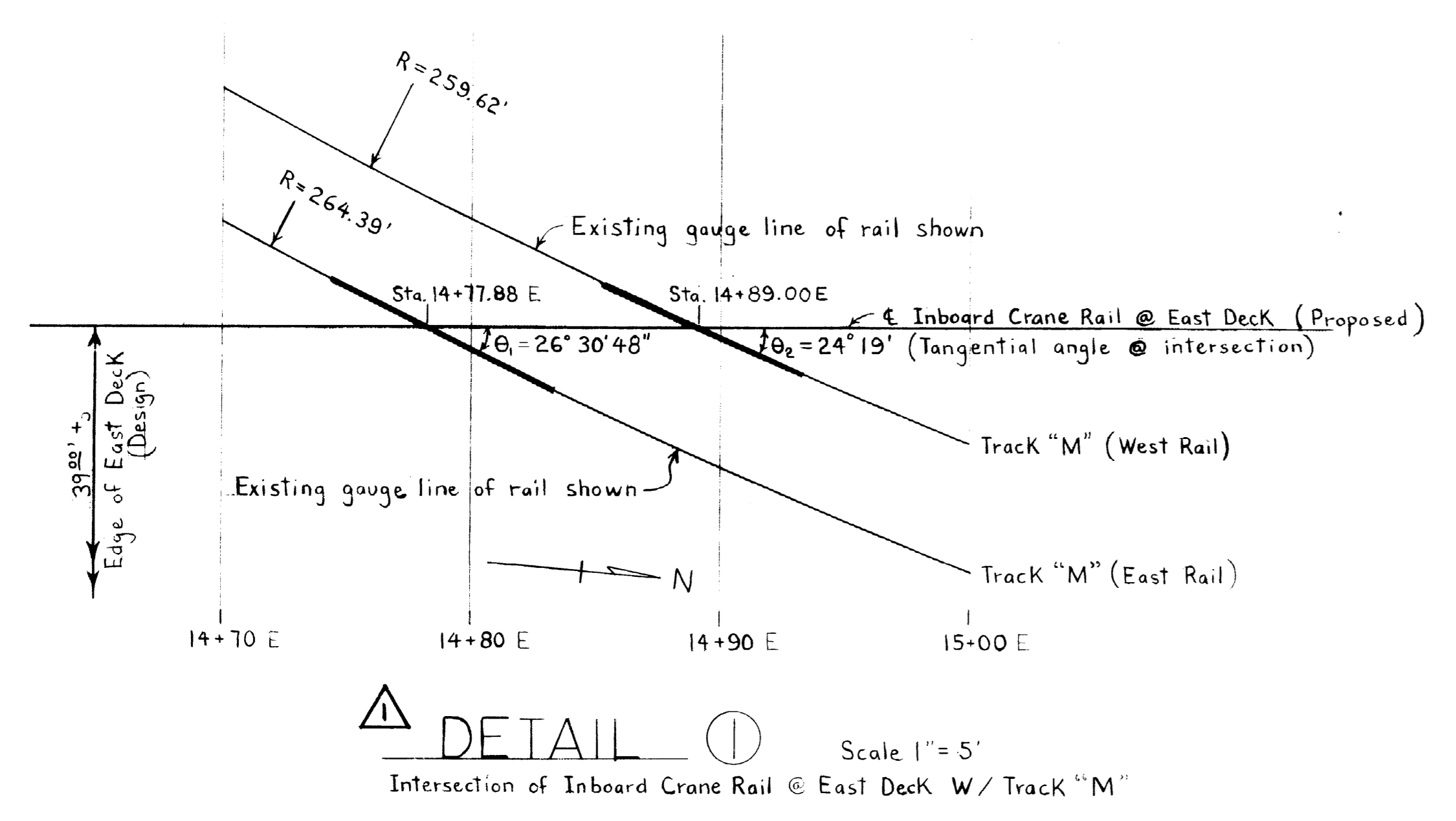
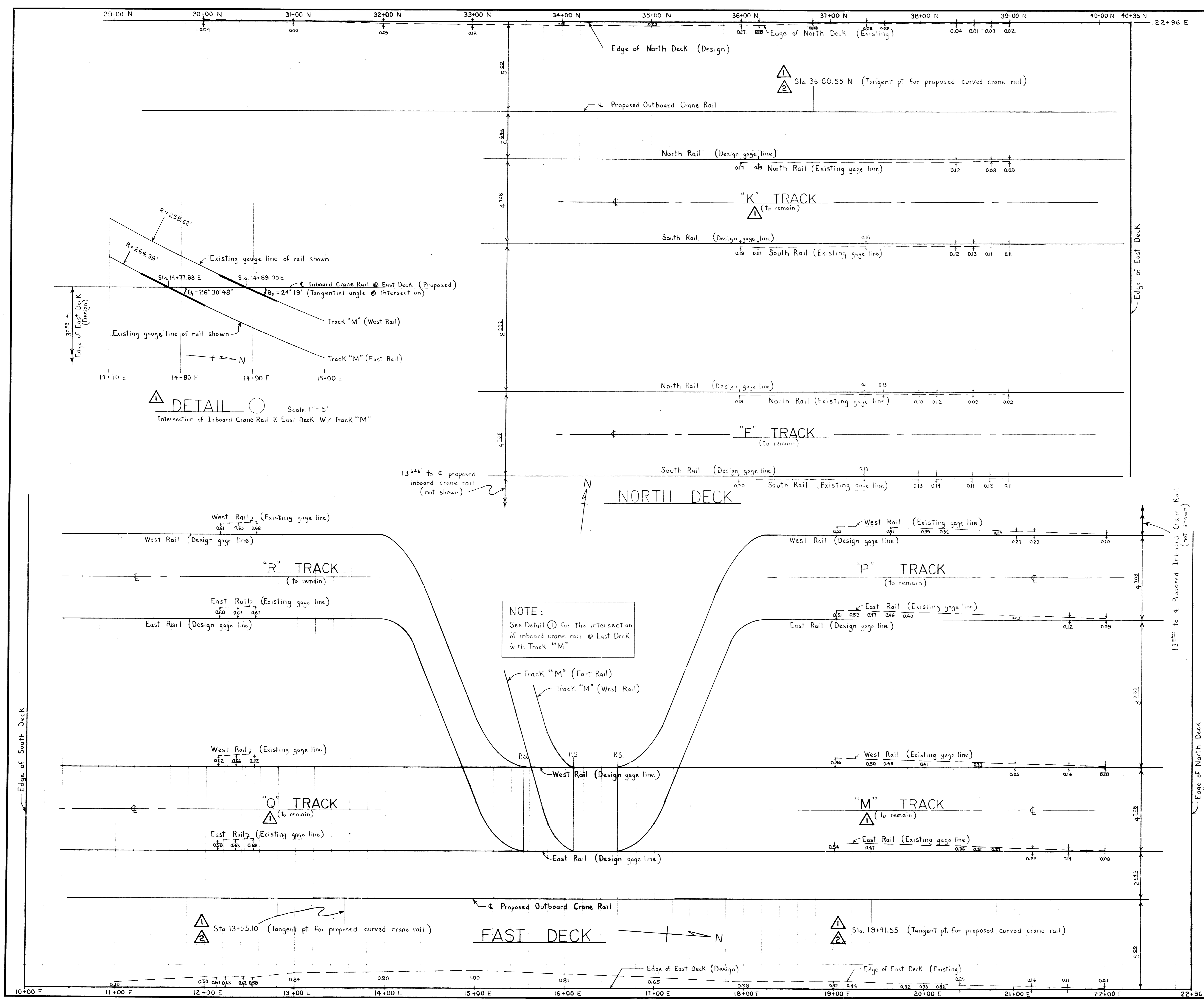
NO.	DATE	DESCRIPTION
10-22-75		Revised Crane Rail Spiral
8-20-75		Revised Detail Plan & CR Profiles for longer CR Spiral Curve. Profile Track 'M'
5-4-75		Corrected 3.51% Circular Curve 245.31 from 230.55 and 275.95 from 341.22
2-25-75		Added frogs, Tracks 'K' & 'M'
1-7-75		Shortened Crane Rail, North Deck

REVISIONS	
IN CHARGE OF FSN	CHECKED BY FSN
MADE BY KLFM	DATE 7-22-1974
TRACED BY FSN	SCALE AS SHOWN
DATE 7-24-74	
DRAWING NO. 8593-80-1	SHEET NO. 13
	OF 14 SHEETS

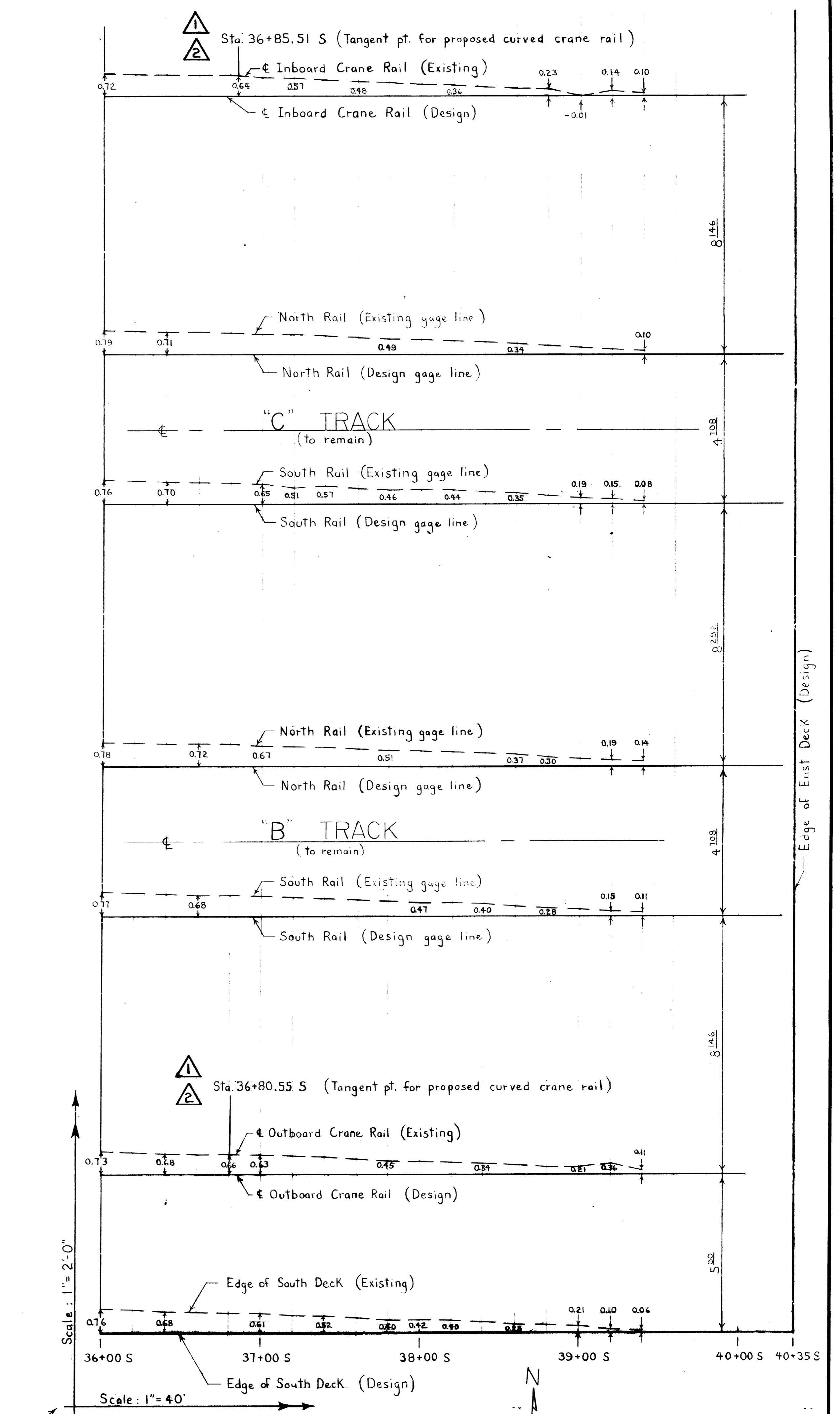
DETAIL PLAN - NORTH HALF
 Scale 1" = 40'

CONTRACT 2519 &
 CONTRACT No 2422

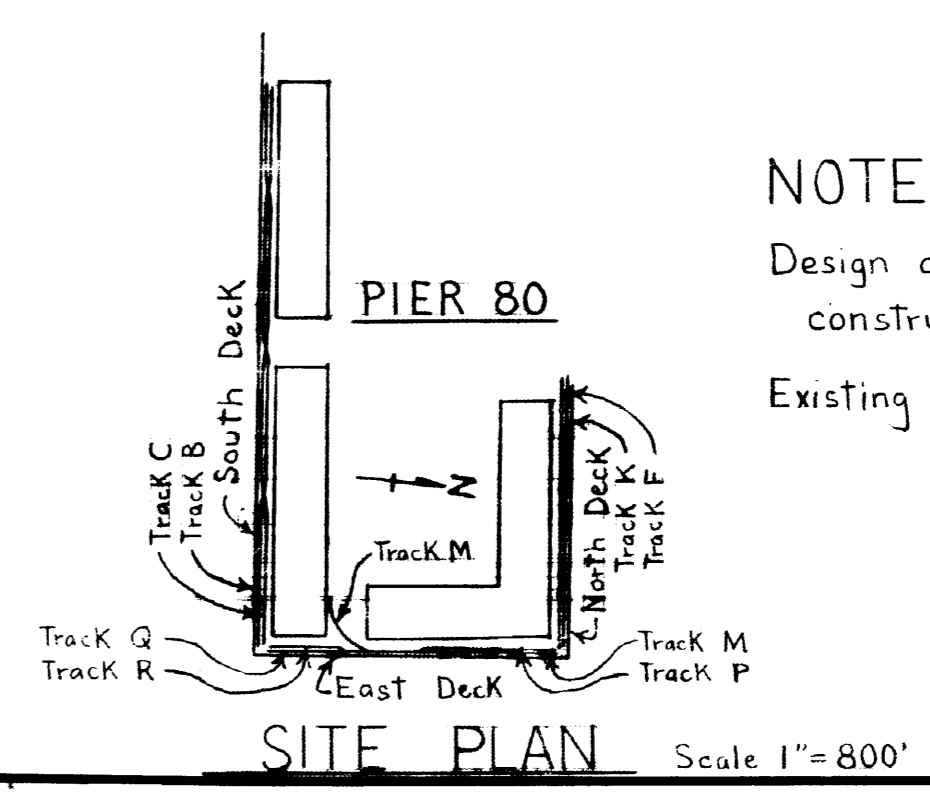
APPROVED BY
 SAN FRANCISCO PORT COMMISSION
 DATE 7-24-74
 C. J. VICKERS
 CHIEF ENGINEER



NOTE:
See Detail 1 for the intersection of inboard crane rail @ East Deck with Track "M"



NOTE: Scale is typical for this sheet except for Detail 1



NOTE:
Design data shown refers to original construction plan Drawing # 7284-86-1 Existing data surveyed 3-74

CONTRACT No. 2422

APPROVED BY SAN FRANCISCO PORT COMMISSION DATE: 7-24-74	CHECKED BY DATE: 3-27-74
MADE BY TRACED BY DRAWING NO.	SCALE AS SHOWN

PIER 80
DECK MODIFICATION
EXISTING RAIL & DECK ALIGNMENT

8594-80-1